
LANCASTER, NY

Preserving the Historic Scale and
Character of the Village Center

School of Architecture and Planning,
University at Buffalo
Date - 30 March 2023



Photo By N.Barbara

ACKNOWLEDGEMENT

The compiling of this report would not be possible without the hard work of University at Buffalo School of Architecture and Planning students in Urban Design. Thank you to the Fall 2022 students of ARC 547 + URP 565 + RED 565 for a semester of diligent work, drawing on many personal, professional, and academic experiences to seek creative solutions in our project area. Thank you to Spring 2022 Graduate Planning Practicum students under the guidance of Professor Ernest Sternberg for setting the precedent of studying the Village of Lancaster through a planning lens. Their holistic approach to identifying gaps in land use provided fundamental questions which we seek to answer through urban design in this report.

To the Village of Lancaster officials, Mayor Lynne Ruda, Deputy Mayor Joe Quinn, and the Board of Trustees; thank you for welcoming us into your beautiful village. Your dedication to public service inspired us to reflect your people-first approach in the suggestions for this report. Lancaster is a unique village with much to offer, and we hope to see its continued growth and prosperity.

DEDICATED TO

The Village of Lancaster

Fall 2022 URP 565, ARC 547, RED 565: Understanding Good Urban Design

University at Buffalo School of Architecture and Planning

Professor Hiro Hata

LIST OF STUDENTS

The Hub (A)

Pratheeksha Balaji, MUP '24

Johnathan Bartol, MUP '24

Michael Marun, MArch '23

Evan Finegan, MsRED '23

The Hub (B)

Shania Anunciacion, MUP '23

Alexandra Kane, MUP '23

James Dam, MArch '23

Trenda Perry, MsRED '23

The Knuckle (A)

Alex Macallini, MUP '24

Viyona Chavan, MUP '24

Meghan Edwards, MArch '23

Evan Gantley, MsRED '23

The Knuckle (B)

Silvi Patel, MUP '23

Jingyao Wen, MUP '23

Peter Bua, MArch '24

John Archilla, MArch '24

The Strip (A)

Zachary Lee, MUP '23

Brian Kwong, MArch '24

Nathaniel Rogers, MUP '24

The Strip (B)

Rene Franqui Jr, MUP '23

Nathan Barbara, MUP '24

Kira Podmayersky, MArch '24

EDITORS

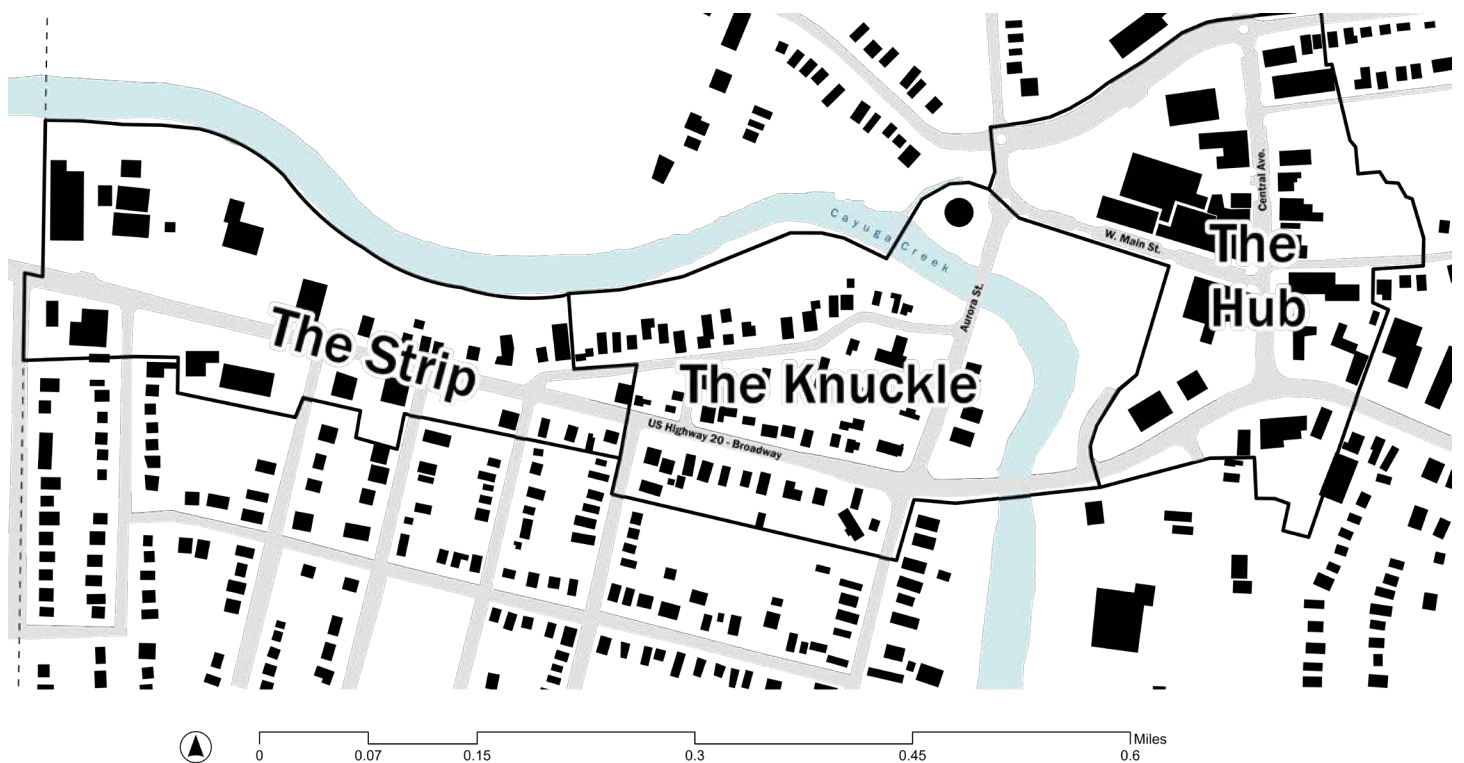
Alex Macallini, MUP '24

Viyona Chavan, MUP '24

Jingyao Wen, MUP '23

TABLE OF CONTENTS

1	INTRODUCTION	6-7
CHAPTER 1	8-13
2.1	TOWN AND REGION	
2.2	SCALE COMPARISON	
CHAPTER 2	14-45
3	FIRST IMPRESSIONS	
4.1	HISTORIC CHARACTER	
4.2	HISTORIC MATERIALITY	
5	INVENTORY and ANALYSIS	
5.1-	LAND USE	
5.2 -	STREETSCAPES	
6	DESIGN GUIDELINES	
CHAPTER 3	46 - 78
7	PROPOSED DEVELOPMENT	
7.1 -	THE HUB	
7.2 -	THE KNUCKLE	
7.3 -	THE STRIP	
8	CONCLUSION	79 - 80
9	REFERENCES	81 - 82



The Village of Lancaster, New York, incorporated in 1849, is a 2.74-square-mile village within the Town of Lancaster in Erie County. The Village offers historic architecture, creekfront views, and a rich history and heritage of fire-fighting. In the 1960s, a fire devastated a large portion of the western face of its downtown area. In its wake, decades of urban renewal practices resulted in sprawling swaths of parking lots within the downtown core. In recent years, the Village has received new hope. An increased concern for the urban form of the Village has seen the recreation of its West Main Street, which was decimated in the fire. There has also been increased focus on the pedestrian experience, with the formation of traffic-calming techniques, and improved pedestrian and cycling safety providing a greater emphasis on the human scale of the village experience. This shift in focus gives Lancaster the promise of a bright future. However, there is still work to be done to preserve the defined historic character of the Village, and increase access to physical endowments including the Cayuga Creek waterfront.

In collaboration with village officials, three project sites were identified that represent unique land uses throughout the village along its major arterial route of U.S. Highway 20 - Broadway. These sites, the Strip, the Knuckle, and the Hub, each represent a challenge that the village faces. The Strip faces sprawl and car-centric planning. The Knuckle faces an undefined character and a missing middle in the transition into the downtown. The Hub has a strong downtown feel but lacks sufficient housing opportunities and a united civic image.

This report will expand on the work of the University at Buffalo School of Architecture and Planning studio provided under the guidance of Professor Ernest Sternberg by advising the Village of Lancaster on potential solutions to urban design challenges they face. The report will compile the work of six groups, each given a defined project site, with the goal of improving the urban design form of the three major sites through urban design guidelines and proposed projects. These project sites will be placed along U.S. Highway 20 - Broadway, mirroring each other to provide solutions along each side of identified target areas. The report will provide graphics created by students that visualize their impressions of the inventory and challenges of the village, as well as their visions for project proposals in the sites.

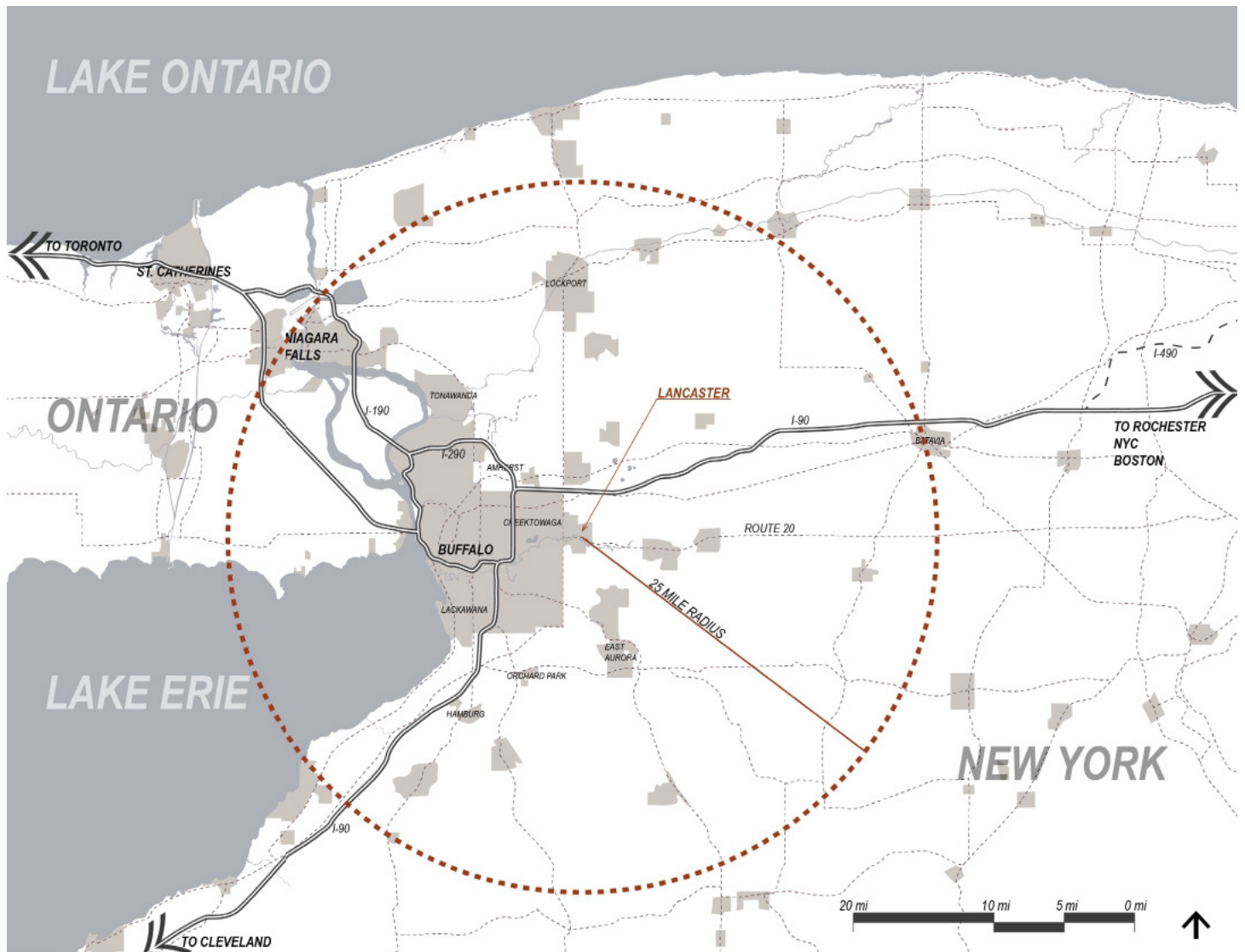
In doing so, the report will provide village officials with a defined vision for the village, as seen through the lens of urban design. This will be provided through a set of urban design guidelines for each of the three major sites. Urban design serves as the professional convergence between the disciplines of Architecture and Planning, which can be utilized to provide a greater understanding of the user's experience of urban areas and create attractive, interesting, and vibrant public spaces. Placing this emphasis on Lancaster will allow for an improved urban character and a greater vitality of new and existing projects throughout the village.

This report seeks to understand the existing and historic conditions of the village and surrounding villages to best advise these urban design guidelines. A precedent study of surrounding villages will provide an understanding of the benefits and demerits of urban forms not utilized in the project sites of Lancaster, and analysis of the historic urban form and architectural material will allow urban designers to understand the features of the village which make it unique. This will be cross-referenced with the post-urban-renewal state of the project sites to identify means of returning this character to the sites. The report will detail the status of right-of-ways, land uses, and user experiences in their current state. These factors will be used to create project proposals by each of the six groups, as well as the design guidelines for each of the three sites. Our goal is to advise projects and guidelines that preserve the historic character of the village and continue the focus of the user experience throughout the village

2.1 TOWN AND REGION

Scale comparisons will be used to give the reader a better understanding as to the Village of Lancaster's scale and character in the context of surrounding villages. Villages for comparison were chosen for similarities and differences to the Village of Lancaster that best explain the strengths and weaknesses of Lancaster's scale and character. The case studies show that while Lancaster benefits from a concentric urban fabric, it is in need of improved middle density and mixed use zoning to place residents close to services. Linear commercial development along Broadway in the Village of Lancaster is also a point of interest, creating an increased dependence on vehicular traffic

to reach services. These comparisons identified Cayuga Creek, and a strong village center as factors that make Lancaster unique to surrounding villages.



Area Map (J. Bartol)

2.2 SCALE COMPARISON

LANCASTER AND HAMBURG

The Village of Hamburg is comparable to Lancaster for several reasons. They both have similar populations and similar areas and developed because of proximity to major rail lines and major US Highways. These geographic features meant that both Villages were established with a similar character in architectural style, as they were developed in similar periods. They are also similar in their urban characters. Both have recently adopted traffic circles as a means of traffic calming, which indicates a step in the direction of pedestrian safety and movement being prioritized. While this may be a step in the right direction, it still indicates the desire of villages to maintain vehicular transportation as a means of transit. Both villages have limited accessibility by public transportation, and as such are reliant on cars to make required trips.

Both have defined downtown cores like the Hub, linear strip developments like the Strip, and single-use-detached development in a transitional zone like the Knuckle. Hamburg's downtown is more sprawling than Lancaster's, with the downtown features spanning several streets. This differs from Lancaster, which has most downtown activity concentrated in a small plot. Hamburg's strip development is also more sprawling, spanning 2 separate streets, and expanding beyond the village boundaries into the town. This differs from Lancaster, which has one defined commercial strip that leads into the Village of Depew, out of the Town boundary. Lancaster holds an advantage over Hamburg with respect to the proximity of these sites, however, they need to place a higher emphasis on the connectivity of their sites.



Lancaster (A. Macallini)

Hamburg (A. Macallini)

2.2 - SCALE COMPARISON

LANCASTER AND HAMBURG

The site comparable to the Knuckle in The Village of Hamburg has segregated land use in each plot. However, the order is more organized than in Lancaster, with commercial uses exclusively held on the arterial roads binding the site. This differs from Lancaster, which has commercial uses scattered throughout the Knuckle. However, this means that Hamburg's site has a lower concentration of pavement than Lancaster's because the commerce is more centrally located. Lancaster has a more interesting views-hed, as the Operahouse, the Watertower, and the Creek viewable from the Knuckle, whereas Hamburg has no defined viewshed in the site. Lancaster also has a more organic street layout whereas Hamburg has a more rigid grid network in the site. These factors give Lancaster's site a higher potential for wayfinding capacity and navigability.



Lancaster (A. Macallini)



Hamburg (A. Macallini)

2.2 - SCALE COMPARISON

LANCASTER AND EDEN

Town of Eden can be compared to the village of Lancaster in that it is a small town that has cultural significance. There is a lower density in the town, allowing for more space but also a more car centric town. Roadways are long and don't have things for pedestrians to access so there is little to no street life.

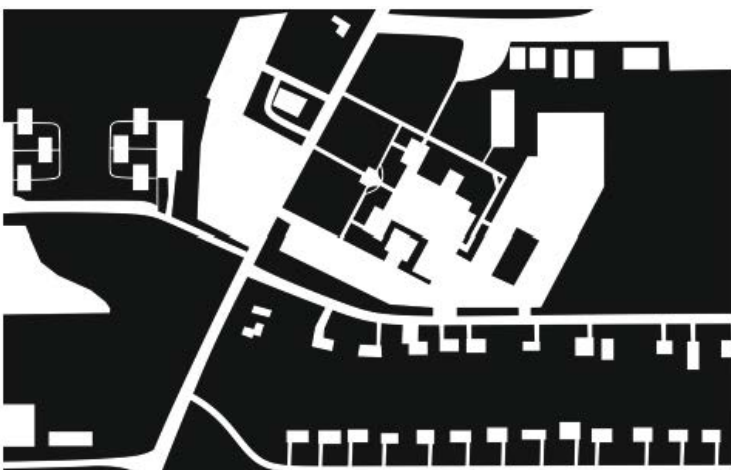
The Village of Lancaster can use Eden as an example of increasing the amount of greenspace, but keep the pedestrian in mind. As it stands now, Lancaster's density is far superior to Eden's and this is something that should be encouraged and enhanced. Lancaster should aspire to achieve the green space of Eden, but



Eden Figure Ground Map (M. Marun)



Lancaster Figure Ground Map (M. Marun)



Eden Greenspace Map (M. Marun)



Lancaster Greenspace Map (M. Marun)

2.2 - SCALE COMPARISON

LANCASTER AND KENMORE



Kenmore (P. Bua)

- K/PROS:**
- DENSE BLOCKS
 - WALKABLE
 - GRID HOUSING LAYOUT
 - CENTRAL LOCATION

- K/CONS:**
- VACANT PARCELS
 - URBAN SPRAWL
 - OVERSIZED ROADS

- L/PROS:**
- WATERFRONT
 - CENTRAL AMMENITIES

- L/CONS:**
- VACANT PARCELS
 - URBAN SPRAWL
 - NOT WALKABLE



Lancaster (P. Bua)

LANCASTER AND PERRY



Village Center of Lancaster (N. Barbara)



Village Center of Perry (N. Barbara)

2.2 - SCALE COMPARISON

LANCASTER AND ALDEN

The town of Alden is a nearby town with comparable conditions to the Village of Lancaster. The first notable difference between the two communities is their grid patterns. Alden has a very regular grid that was slowly filled and redeveloped with time, while Lancaster's is an unusual grid that has been shaped by the creek and warped by the destruction wrought by the Fire of 1970. Alden's more standard development has allowed it to develop more new green space and parking centered around green space and the school. New parking was placed in Lancaster after the fire, causing the hub area to have more parking than other comparable towns. Lancaster also has far more automotive transit oriented developments, with a major suburban commercial strip and two major bus routes. Alden's roadways have a green median strip and streetside parking which help work to create a slightly more friendly environment. Their street signs are in far worse condition than Lancaster's though, worsening conditions for motorists. Lancaster does well supporting motorists with large amounts of parking, wider roads, and bus routes, but could learn more about other forms of infrastructure from Alden. Recent street improvements in Lancaster's hub area show the village knows what needs to be done and that it is moving to improve local conditions.



Village of Alden (Z. Lee)



Village of Lancaster (Z. Lee)

3

FIRST IMPRESSIONS

SKETCHING EXERCISE

Sketches were created on the project team's first visit to Lancaster to capture first impressions of the village. The team's attention was primarily drawn toward the village center and the views of West Main Street. The stark contrasts between the east and west sides of the street were particularly noteworthy as the east side's historic buildings abruptly end halfway down the street, giving way to vacant lots and wide-open views of the water tower. The stock of surviving historic buildings also caught the team's eye with excellent examples of well-preserved turn-of-the-century commercial architecture. Also noted downtown was the potential underutilized spaces had, not just the vacant lots but the space between buildings as well as holding potential for unique urban moments.

An initial analysis of the street conditions was also undertaken throughout the major streets of the village. Street section studies proved to be very informative in illustrating the urban character of the village. Downtown streets were observed to be much more pedestrian friendly, well landscaped, and felt much more like defined urban spaces. In contrast to this, the conditions on Broadway gradually worsened while traveling west from downtown. The beginning blocks of Broadway west of the creek have a very residential suburban character with wide, but not overly oppressive, streets and a density appropriate for an area of single-family homes. This character very quickly transitions to a commercial suburban character with extremely wide and oppressive streets and deep setbacks, as well as a severe drop in building density. The team did not make sketches from this area as it is not a very comfortable place to be and does not feel like a safe street to walk along. experience.



Main Street (J. Bartol)



Main Street (P. Balaji)



Broadway-Central Intersection (S. Julia)



Viewing down Central Avenue (S. Julia)

3 - FIRST IMPRESSIONS

MIXED IMPRESSION DETAILS

Lastly, observations were made of the work the village had done on the streetscape downtown. Bump-outs for pedestrian crossings, ample landscaping, and the use of decorative pavers all work together to create a much more pleasant pedestrian experience.



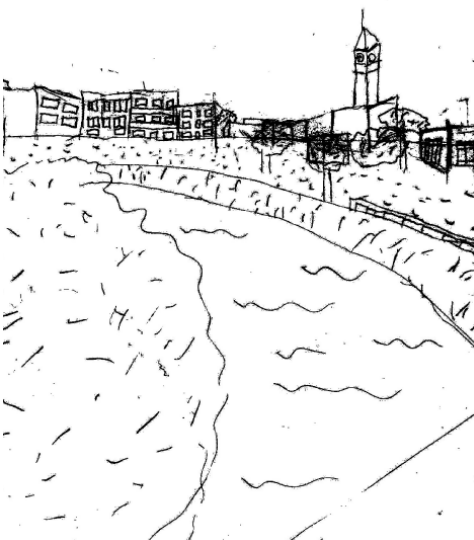
Central Ave. alleyway (N. Barbara)



Cayuga Creek off Broadway (N. Barbara)



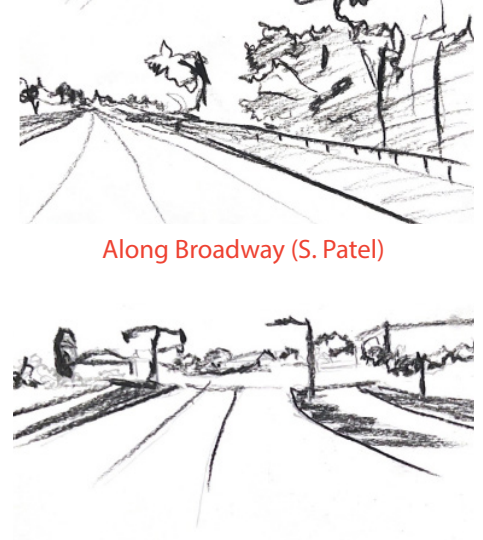
W. Main St. Spine (N. Barbara)



The Creek (Z. Lee)



The Strip (Z. Lee)



Along Broadway (S. Patel)



Along Broadway (S. Patel)

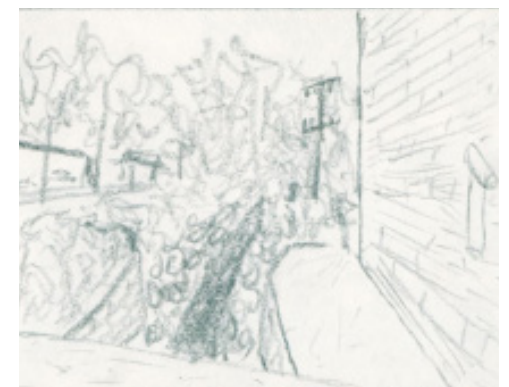
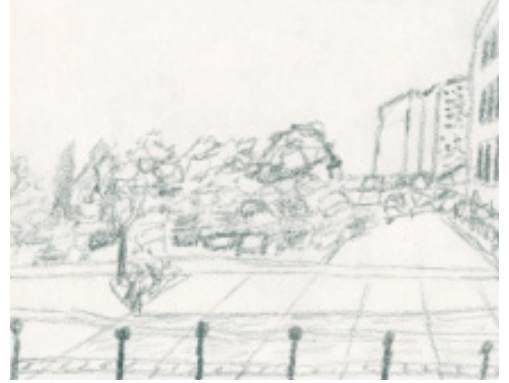


Along Broadway (S. Patel)



Along Broadway (S. Patel)

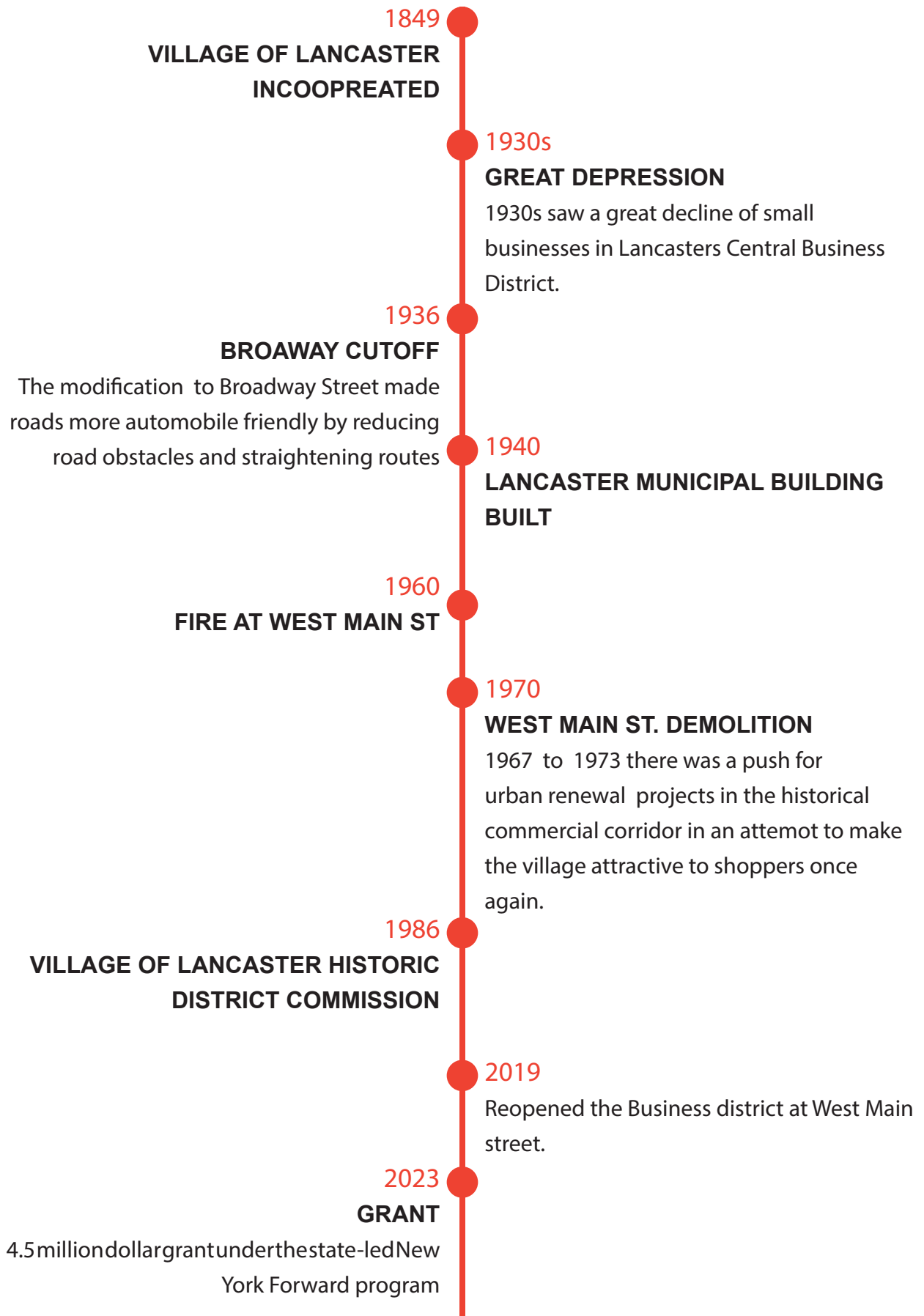
3 - FIRST IMPRESSIONS



Sketches (N. Rogers)

4.1

HISTORIC CHARAC-



4.1 - HISTORIC CHARACTER



The Village of Lancaster, Late 1800s (A. Macallini)

BEGINININGS

The Village of Lancaster has grown in importance over the past few years and has come to be regarded as one of the most desirable suburbs of Buffalo, the two places have been more intimately connected by street and railroad lines. Originally part of the Town of Clarence, the Town of Lancaster was established as a separate government by an act of the New York State Legislature in 1833. Later the Village of Lancaster would carve itself out from the Town of Lancaster and incorporate itself in 1849. The Village of Lancaster is located approximately eleven miles east of downtown Buffalo, about thirty-seven square miles in area. Milling was the first permanent industry within the area that became the Village of Lancaster. In 1811 Ahaz Alien built a grist mill on Cayuga Creek. By the late 1800s, there was a well-established business district along Central Avenue and West Main Streets consisting of generally two-story wood-frame commercial buildings and dwellings on Central Avenue and smaller, more concentrated buildings along West Main Street. The Lancaster Presbyterian Church at Broadway and two homes are among the few buildings which have been documented as dating from this early period. The earliest architecture in the Village was typified by small-scale residential buildings, including several executed in vernacular adaptations of the Greek Revival style.

Broadway, 1929 (also Cayuga Creek Plank Rd. & Buffalo Street)
 (in rare photo was taken looking west from St. John's Street. The Dutch Mill would be in the recent field to the left. The three buildings in the photo are still standing today. In foreground is 5258 Broadway. The white house is 5350 Broadway, built in 1908. The road in the distance on the left is E1 Interwound Road. Highway was traversed by the Indian White Seneca about 1801, and referenced in 1808 originally surveyed in 1813. LAMARCA COURTESY



Broadway 1929 Photo (Gathered by S.Patel)



Downtown Lancaster NY, pre-1961 fire.

4.1 - HISTORIC CHARACTER

GROWTH AND DECLINE

Central Avenue is home to the original commercial core of the village of Lancaster; the district represents a period of growth and prosperity for downtown in the decades surrounding the turn of the twentieth century, due to industrial growth and increasing ease of transportation. A fire that was to alter the destiny of the entire downtown commercial area occurred on March 5, 1961, when flames were discovered in the rear of the Oelkers Furniture Co. building on the north side of West Main Street. The fire dealt a severe blow to the downtown business community. This led to a large clearance of buildings on the central avenue that gave the village of Lancaster a chance to build its new identity. In the 1960s, many cities that were economically disadvantaged were subjected to urban renewal under the Housing Act of 1949, the village of Lancaster being among them. The fire of 1961 did much of the work that the process of Urban Renewal likely would have done by clearing the majority of downtown of its historic building stock. In their place would be built an urban strip mall that would block the historic street grid and alter the character of downtown for the worse.



Main Street (N. Barbara)

RESURGENCE

Lancaster's nationally registered Historic District covers approximately three-square miles – making it one of the largest urban historic districts in the United States. The buildings within the Central Avenue Historic District represent the remaining historic commercial buildings from the 1860s to the 1930s that survived the urban renewal efforts in the 1960s and 1970s. The district is primarily composed of two-part commercial blocks or two and three-story masonry mixed-use buildings. Architectural elements represent popular styles of the late 1800s, including the round arch windows and modest denticulated cornices of the Italianate style and the asymmetrical decorative facades of the Queen Anne style.

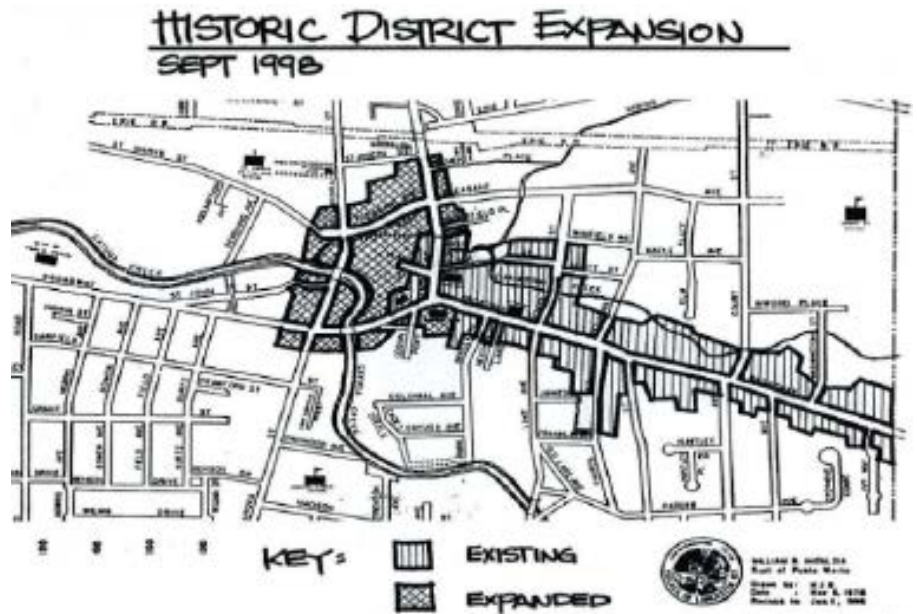
The last few decades have seen renewed interest in Lancaster's downtown district. Already city leaders have made enormous strides in undoing the wrongs of the 1960s and 70s with a restoration of the historic street grid, enhanced streetscaping, and plans for a new park along the creek. Development is occurring downtown for the first time in decades, and there is enthusiasm for what lies in store for the village.



Opera House (N. Barbara)

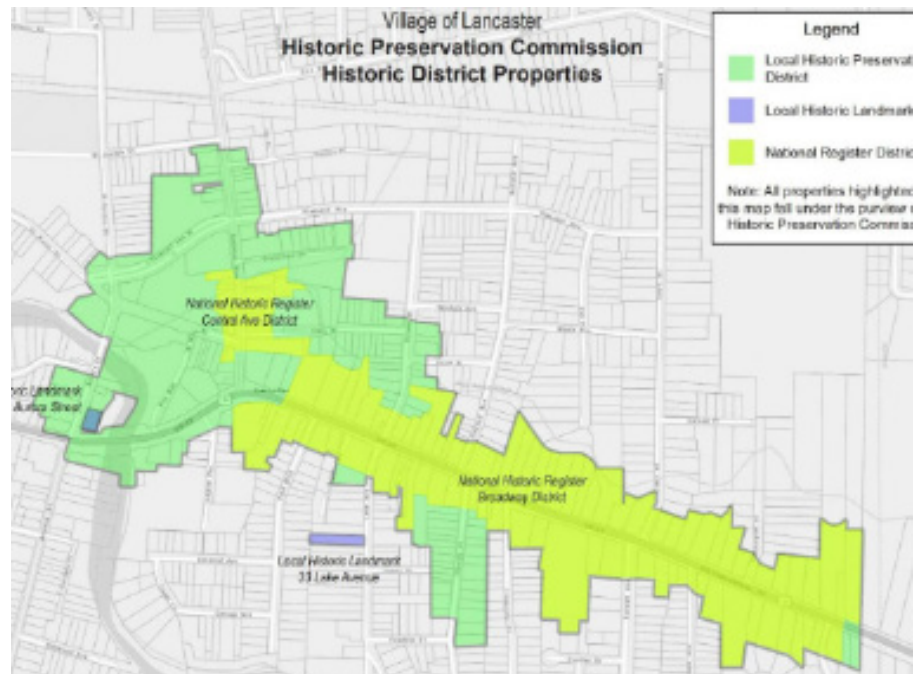
4.1 - HISTORIC CHARACTER

The Historic District of the Village of Lancaster was expanded in 1998 to include a small region beyond the creek. This site is what we are referring to as “The Knuckle” and is a key transformative site for the potential of the Village of Lancaster.



Historic District Expansion Map (Gathered by S. Patel)

Taking advantage of this Historic District Expansion could provide a massive benefit in terms of surrounding Cayuga Creek with economic development and expanding the village center. Additionally it may make projects in this area more economically feasible with historic tax credits.



Historic Preservation Map (Gathered by S. Patel)

4.2

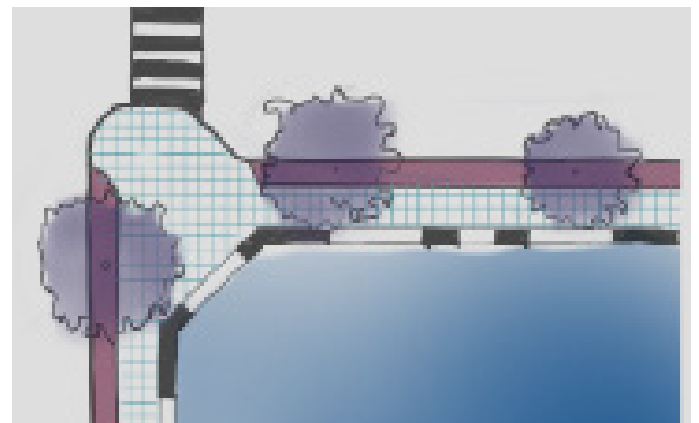
HISTORIC MATERIALI-

GUIDELINES FOR BUILDING MATERIALS

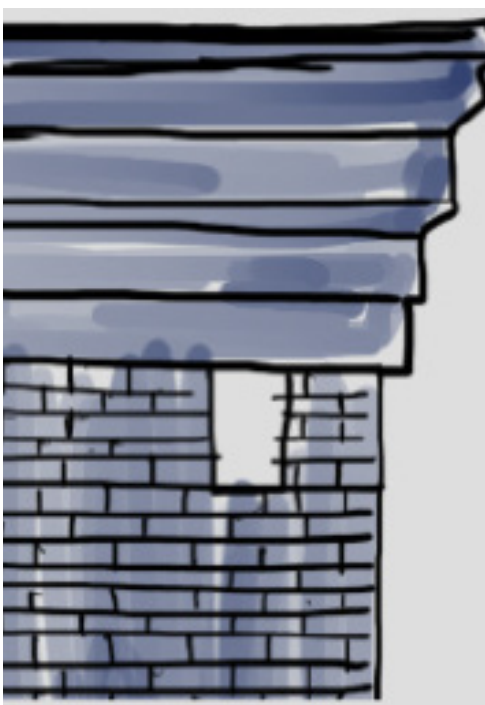
When designing our proposal, we set out to make sure that we created something that fit within the village contemporary style. Brick exteriors, punched apertures, similar scales in height, sidewalk bump-outs, and cornice lines, were all things that we identified as essential architectural and urban design elements to replicate from the Village Center when designing our project. Additionally, seeing that there were minimal outdoor spaces nearby for residents and users to relax, we decided to create an angular void massing in the building form to allow for a small green space for people to enjoy. We believed this decision did a few things including: creating outdoor space, activating the street, encouraging interaction, drawing attention from visitors and allowing for the contemporary curtain wall moment to let light to enter the building.



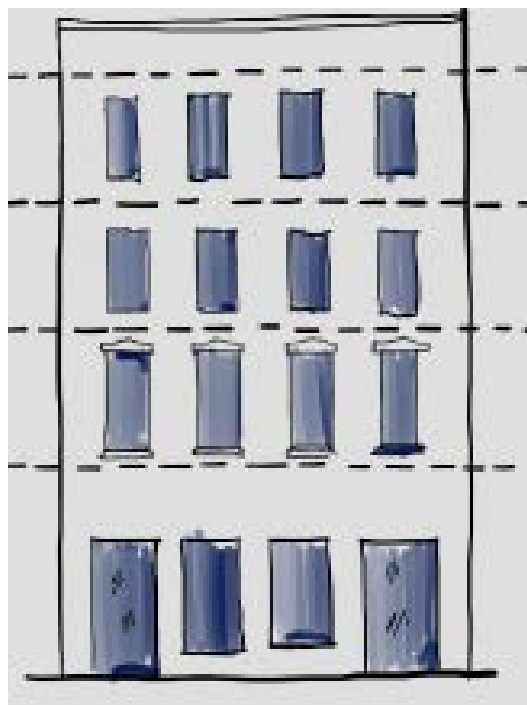
Mansard Roof (S. Patel)



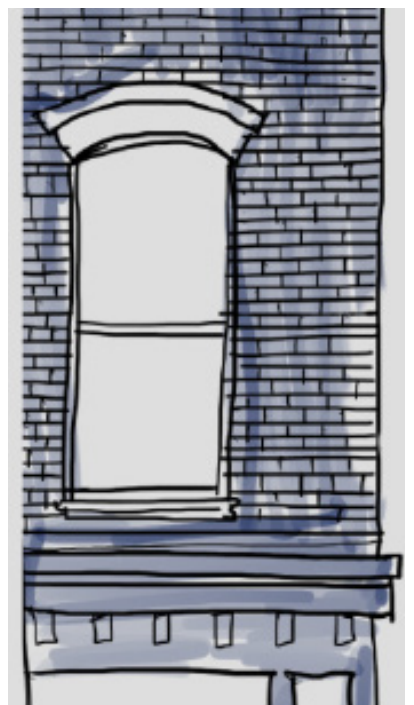
Bump-Out Detail (S. Patel)



Cornice Detail (S. Patel)



Historic Context Scale (S. Patel)



Punched Aperture Detail (S. Patel)

4.2 - HISTORIC MATERIALITY

Careful consideration was applied to the historic materiality of the village, particularly in the Hub, which holds a unique character. Understanding key architectural components of the hub will allow for a better advisement of the design of new projects in the village. This practice was observed in new mixed-use development on West Main Street, which took inspiration from traditional downtown architectural styles. While painted wood siding and shingling are prominent in the hub, red brick is the most defining feature. Featured in some of the most prominent buildings including the opera house and The New York Store. It is also featured in street-level ordiementation including the welcome sign on the corner of Central Ave and Broadway.



1898 Photo (S. Patel)



1919 Photo (S. Patel)



Existing Black Sheep & Co. (M. Edwards)

5.1 LAND USE

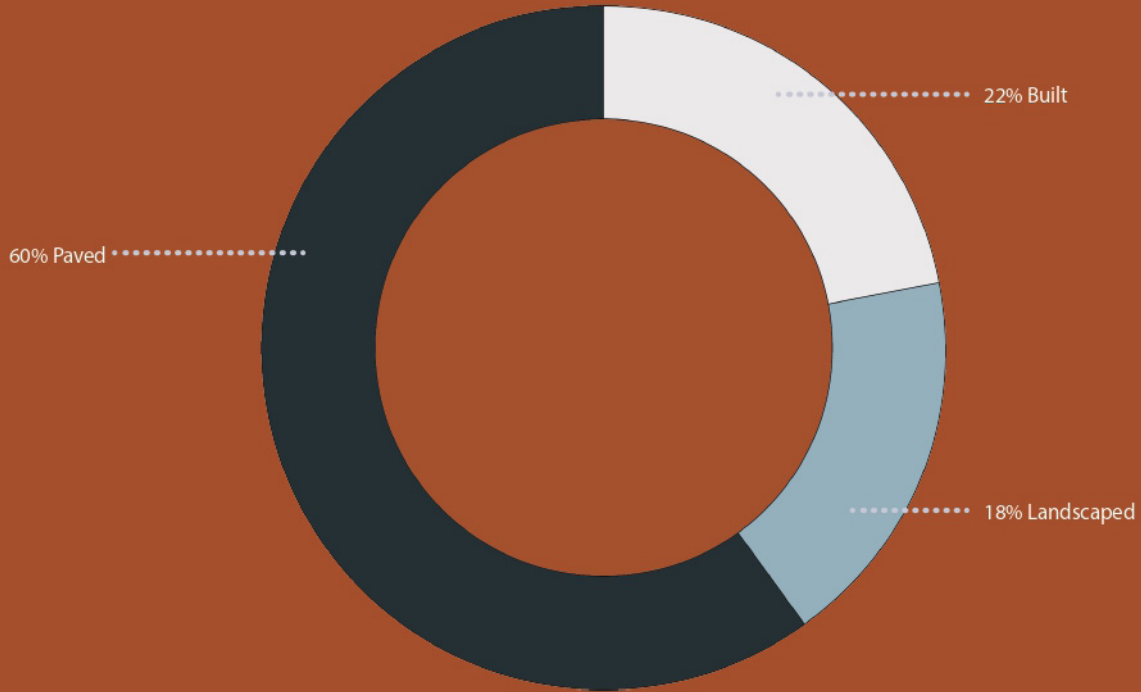
THE HUB - EXISTING CONDITIONS

The current conditions of Downtown Lancaster are a direct result of its history of destruction and the lasting effects of Urban Renewal. This plan identifies the boundaries of Downtown as Pleasant Ave to the north, Cayuga Creek to the west, Broadway to the south, and the parcels fronting Central Ave to the east. The boundaries also include the village hall and its adjacent parcels, the parcels immediately to the east of the Lancaster Opera House, and the two parcels immediately east of the current Sunoco Gas Station. The entirety of the Central Avenue Historic District is located within these boundaries, as well as the western end of the Broadway Historic District.



Current Conditions Map (J. Bartol)

5.1- LAND USE



BUILDING STOCK

37 Buildings

1 Mixed-Use

27 Commercial

5 Institutional

4 Single-Family Housing

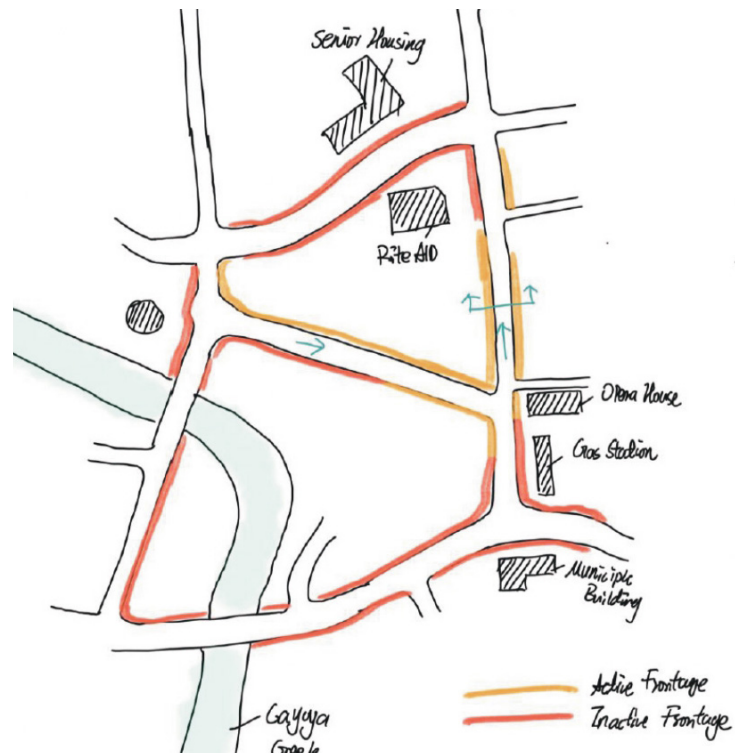
18 Apartment Units

5.1- LAND USE

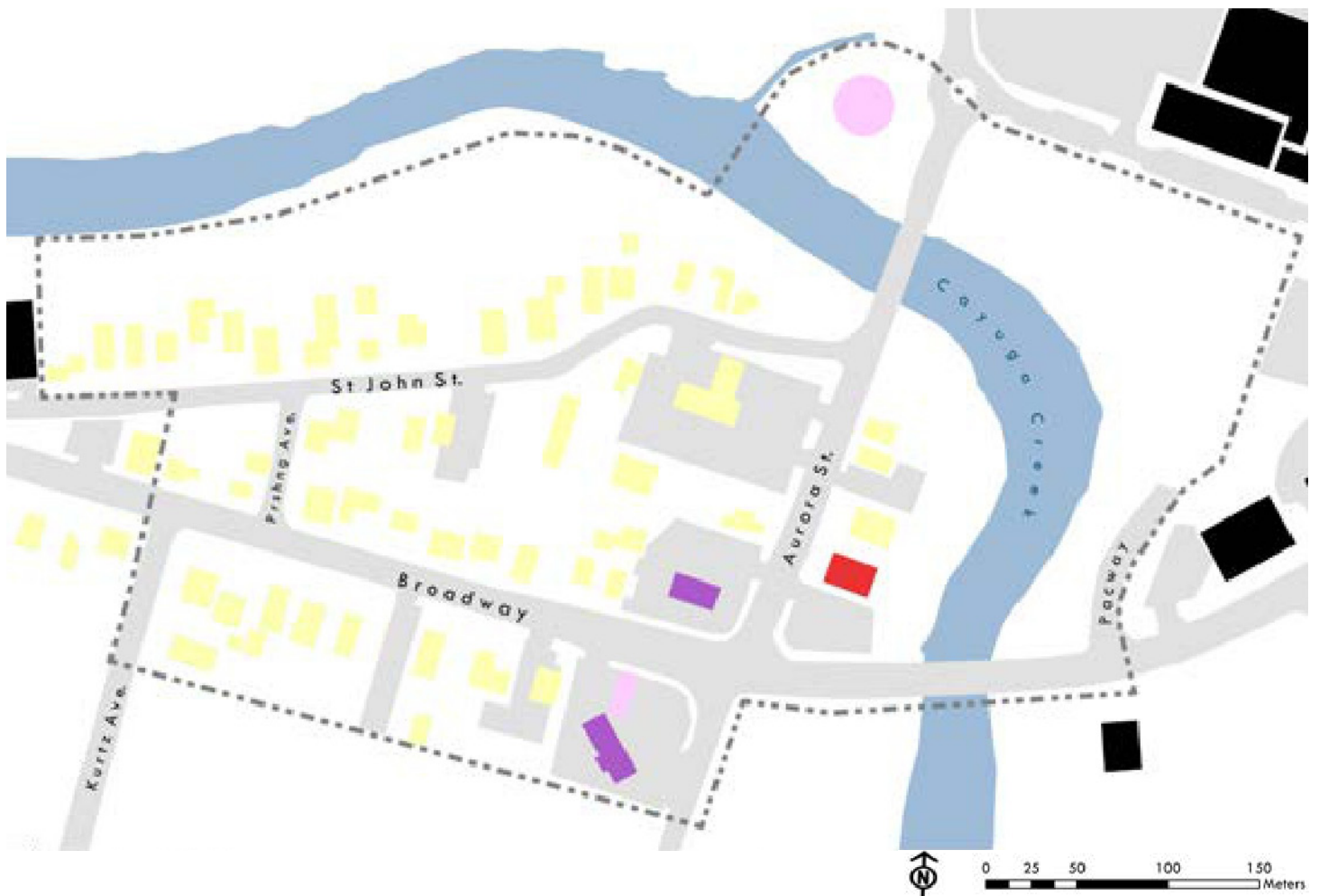
THE KNUCKLE - EXISTING CONDITIONS

Analyzing the Knuckle, especially in comparison to the surrounding sites, the Strip and the HUB, the site itself is predominantly residential aside from a few buildings along Aurora St. along both sides, mixed with staggered residential between them. There is an apparent lack of transition of building use not only on the site but also when transitioning between the three sites, and given the Knuckle being that bridge between the Strip and the HUB, going forward it will be important to make that transition more fluid in order to activate the site.

Lancaster has a fairly good level of activity along the frontage of the buildings in the Village Center. However, adding more frontage will increase economic development and create incentive to activate the streets further.



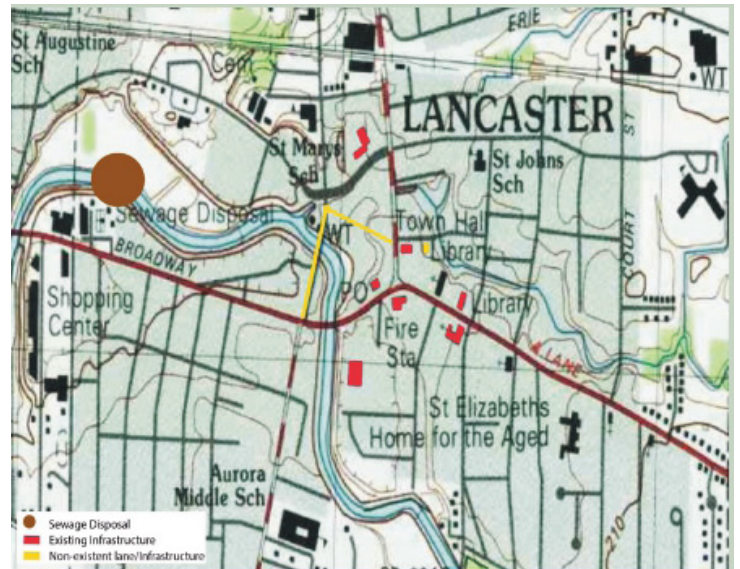
Frontage Accessibility Map (J. Wen)



Land Use Map (A. Macallini)

5.1- LAND USE

The 1982-1983 map of Lancaster to the right shows more recent conditions around the site. Lancaster's sewage disposal was originally located near Cayuga Creek, which could have impacts on people who use the area. The sewage disposal has now moved to 3327 Walden Avenue in Depew and the sewage department was relocated nearby at 3789 Walden Avenue. The berm (highlighted in brown) works to protect the area from flooding, and is still a highly visible piece of infrastructure along the waterfront to this day. Other notable pieces of infrastructure located within the surrounding area are: the town hall, fire station, library, Lancaster Tower, Shopping center, and post office.



1982-1983 Map (Z. Lee)

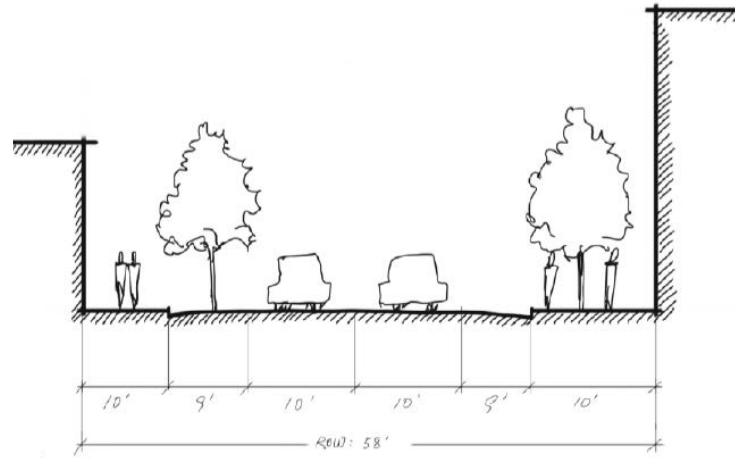


Zoning Map Diagram (J. Archilla)

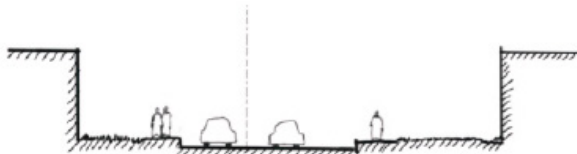
5.2 STREETSCAPES

Within the newly expanded Historic District are key sites now falling under the Traditional Business District zoning code. With the Lancaster building code having leniency in terms of new construction in the Traditional Business Zoning it should be quite easy for developers to see the potential in these sites for future mixed-use buildings.

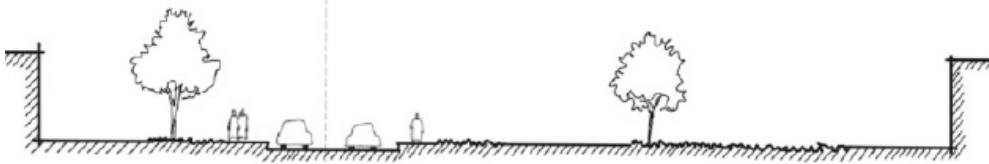
The Village Center has great Right-Of-Ways with pedestrian friendly sidewalks, greenery, and parking spaces to provide a comfortable barrier between the streets and the pedestrians.



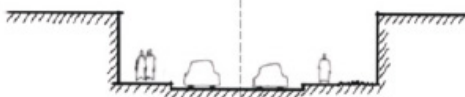
F: Center Ave at the Hub



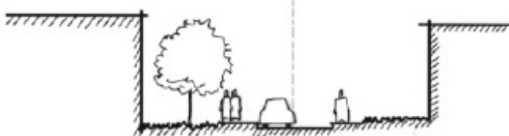
A: Broadway at the Strip



B: Broadway at the Strip



C: Broadway at the Knuckle

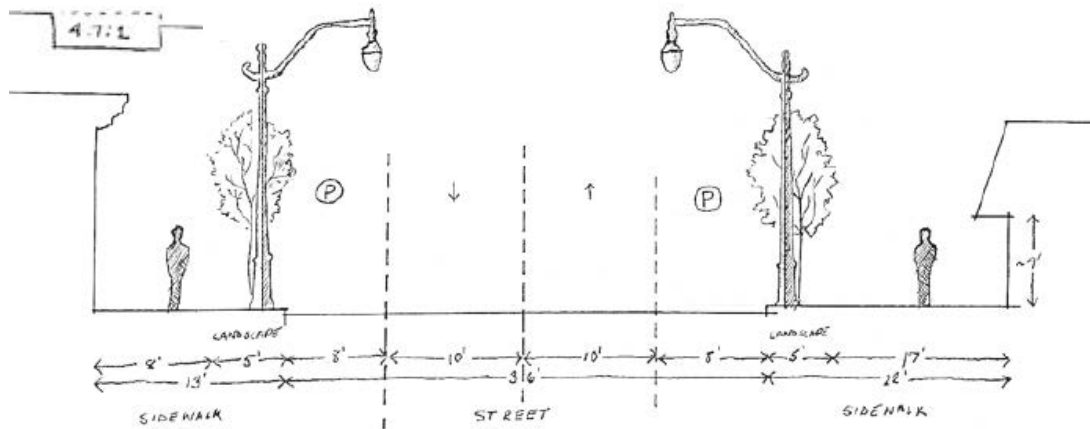


D: Broadway at the Hub

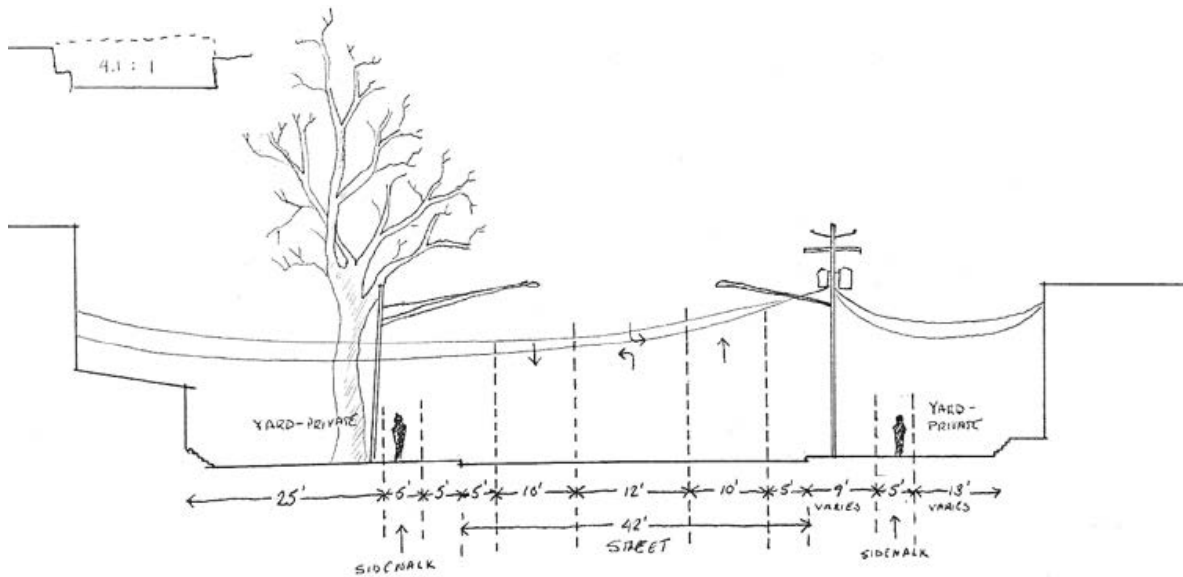


Right-Of-Way Analysis (J.Wen)

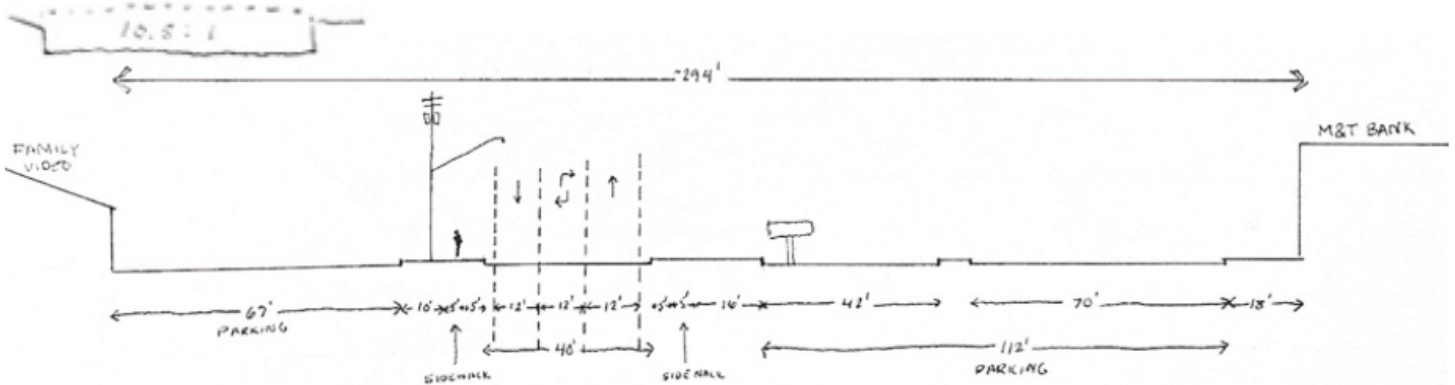
5.2- STREETSCAPES



Downtown Street Section (J. Bartol)

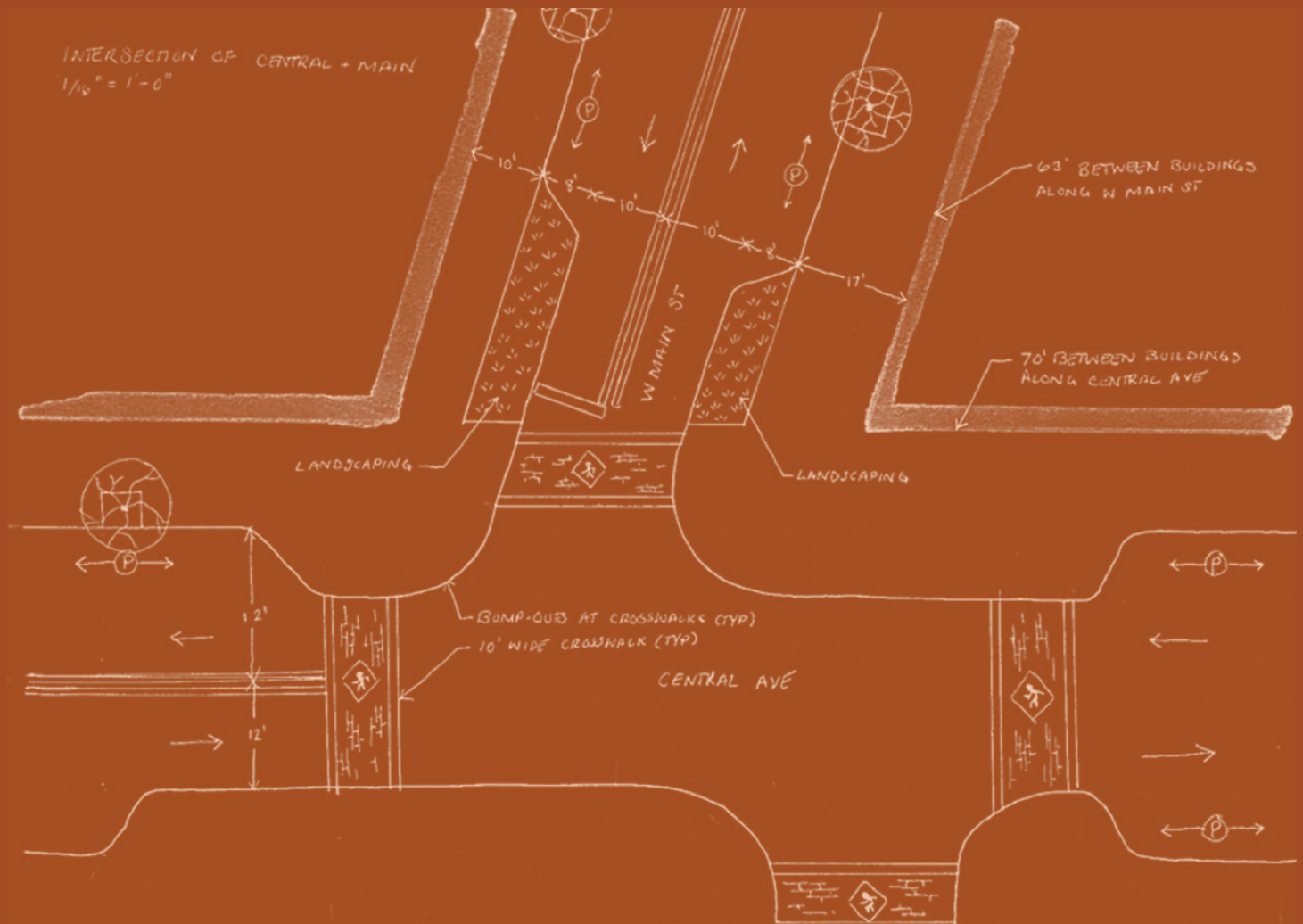


East Broadway Street Section (J. Bartol)



West Broadway Street Section (J. Bartol)

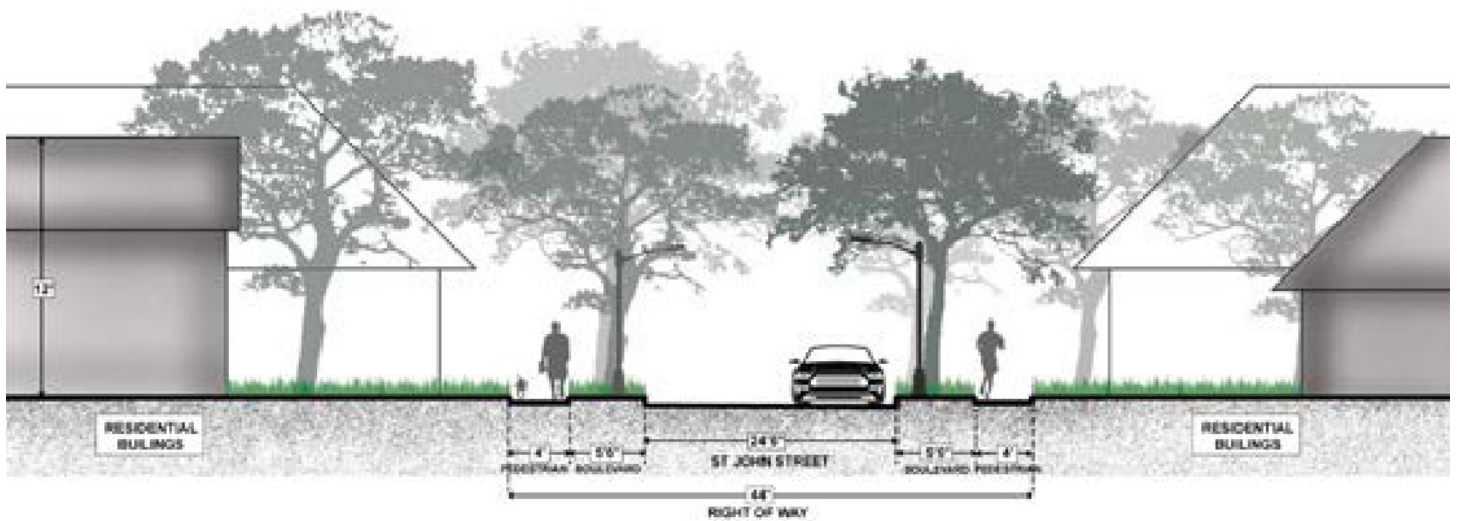
5.2- STREETSCAPES



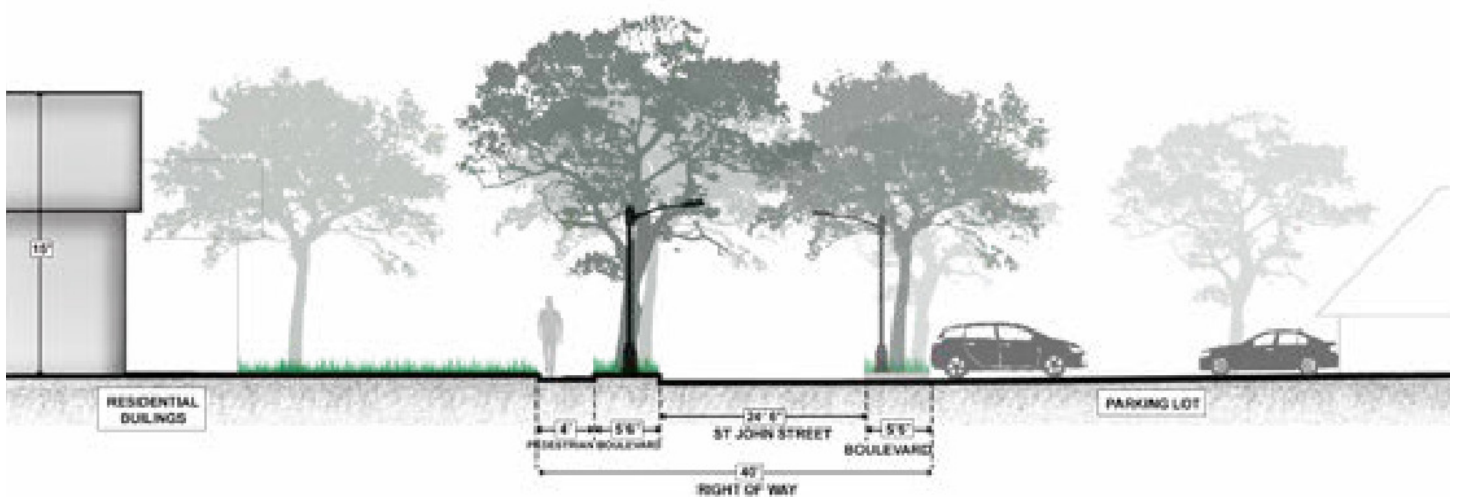
Intersection Diagram (J. Bartol)

5.2- STREETSCAPES

The Section analysis was done to study the character and scale of the Knuckle. It helped in studying the right of way along St. John Street and its comparison with the heights of the residential houses. The height ranged from 10' to 15'



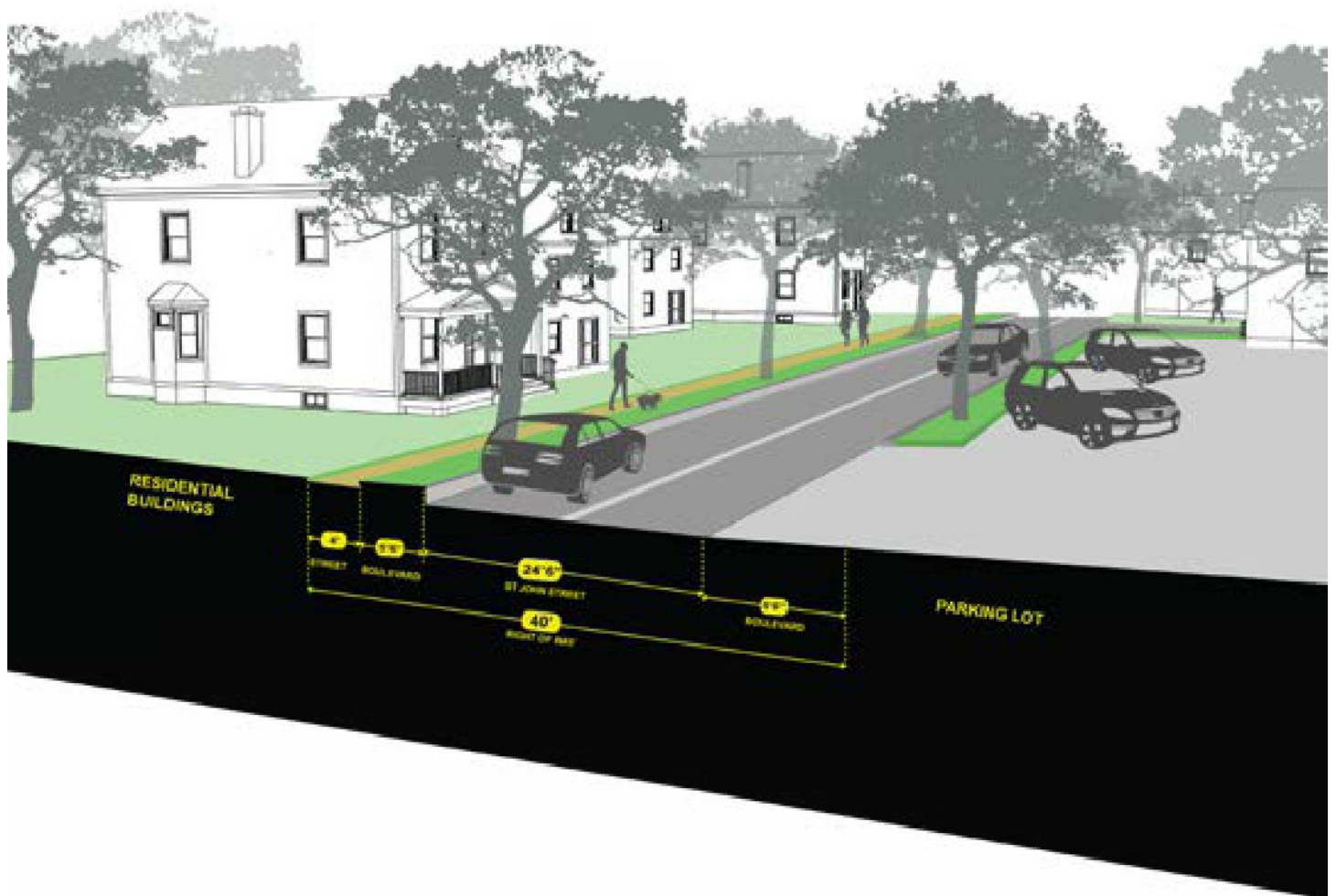
St. John Street Section (V. Chavan)



St. John Street Section (V. Chavan)

5.2- STREETSAPES

The axonometry of the existing character of the St John street and the proposed site at Aurora st helped in defining our vision and identifying the character of both the streets and how do they complement each other without making the proposed site look alien or out of space from the neighboring residential hub



St. John Street Sectional perspective (V. Chavan)

VISION

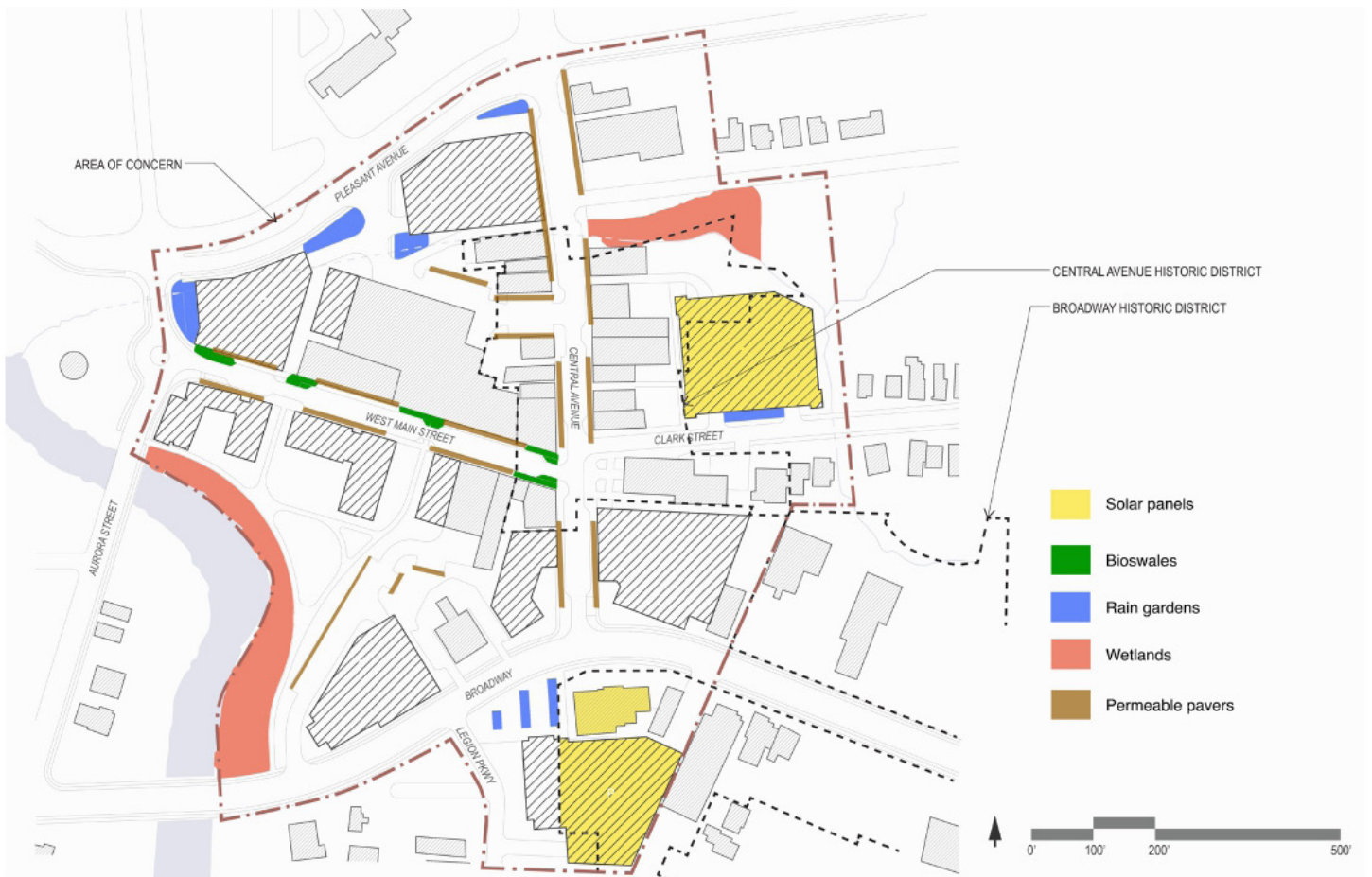
The guidelines set out in this document aim to lead future development in the Downtown District of Lancaster to be conscious and respectful of the historic context of the district. The goal is not to recreate historic architecture, but instead to emulate it in craftsmanship, mass, and materiality. New developments will employ modern technologies and be built to suit modern needs; but in utilizing the same language and materiality of the district's architectural and urban design, the district will hopefully be able to reclaim the character lost in the 20th century while also keeping an eye on the future.

ENVIRONMENTAL GUIDELINES

FOR MAKING DOWNTOWN OF LANCASTER SUSTAINABLE

Downtown Lancaster has many opportunities to employ green infrastructure methods. This plan proposes that anywhere surface parking is located be paved with permeable pavers rather than asphalt to help mitigate rain water runoff. Bioswales located in the road verge as well as rain gardens in the new public plazas and landscaped areas also assist with this effort.

The area of Plum Bottom Creek is an excellent opportunity for a second downtown park. This park could consist of enhanced or restored wetland conditions as well as educational signage or other opportunities to help educate residents about the benefits of green infrastructure and landscape restoration.



Planning for Sustainable Downtown Map (P. Balaji)

ARCHITECTURAL GUIDELINES

MASSING

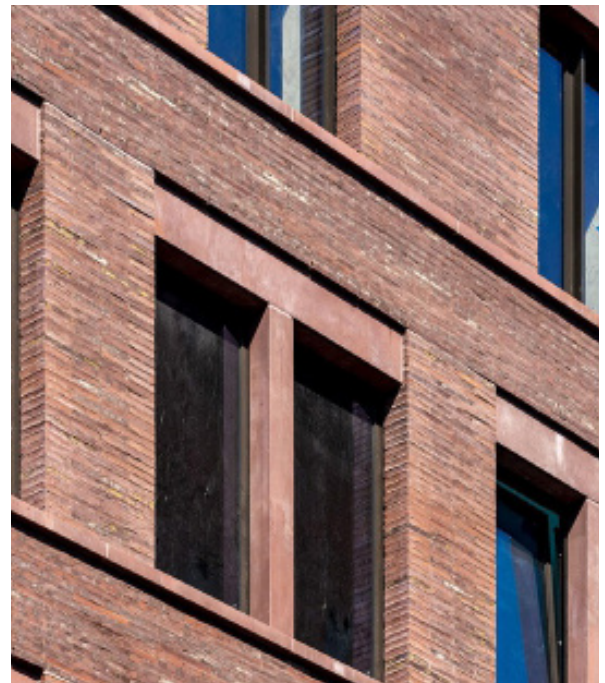
1. No new development over 50' (4 stories) is permitted so as not to detract from the prominence of the Opera House.
 - When 4 stories are proposed, it is encouraged that the top floor is of a mansard roof design.
2. New development must be built to the street property line.
 - Corners on intersections are encouraged to be chamfered and ideally used as entrances.
3. Building Design must include a distinct base, middle, and top.



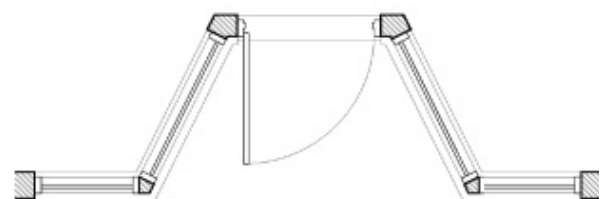
Mansard Roof

FACADE DESIGN

1. Glazing.
 - Windows are to be punched and vertically oriented.
 - Ground floor street facades must have no less than 50% glazing.
 - Upper floor street facades must have no less than 20% glazing and no more than 50%.
 - Total street facade glazing must be no less than 30% of the total area.
 - Windows should be recessed into masonry.
2. Entries.
 - Primary entries are to be recessed.
 - Recessed entry alcoves.
 - Colonnades
 - Emergency egresses may be flush with the facade.
 - Parking Entry;
 - Overhead doors to parking garages must have glazing.
 - Parking garage entries may not be located along main street facades on West Main Street, Central Avenue, or Broadway.
 - Overhead doors must be recessed from the street front property line by no less than 4'.



Recessed Windows



Entry Alcove

ENVIRONMENTAL GUIDELINES

SOLAR

Basic analysis shows 116 3x5 photovoltaic panels can fit on the roof of Lancaster's Village Hall. The village's municipal buildings used 239,268 kWh of electricity in 20191. The average energy production per panel in New York State is 1.2 kWh/day2. Thus, in order to offset energy costs by municipal buildings, the village would need roughly 600 panels. The Village Hall is a prime location for PV panels, with an estimated capacity of 116 panels on the roof. Further panels could be placed on the second floors of the proposed parking structures, or over existing parking lots both as shelter and for energy production. Both areas have open southern exposure that would make them prime targets for downtown solar arrays. Solar arrays pay for themselves within years, eventually providing free energy. Standard warranties are 25 years and long term savings are significant. Government grants and incentives could also bring down the initial costs of solar arrays. Lastly, through NYSERDA's clean energy communities program, after completion of a number of high impact actions such as installation of solar arrays, a municipality receives

ALLOWABLE PANELS

1,416

Panels can fit on the second floor of the proposed north parking structure.

1,074

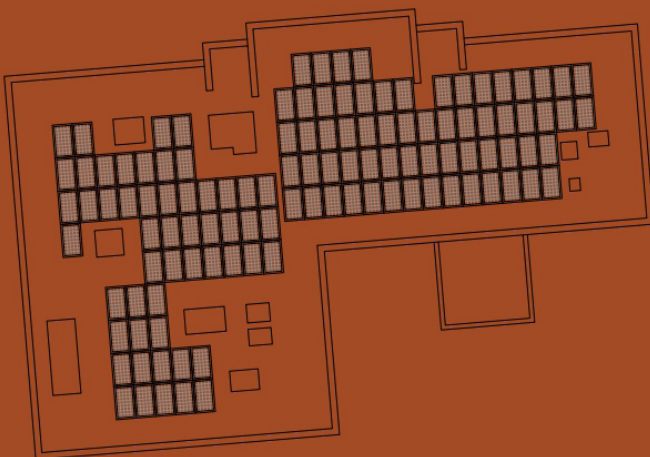
Panels can fit on the second floor of the proposed south parking structure.

116

Panels can fit on the roof of the existing Village Hall.

139.2 kWh/day

50,808 kWh/yr
Produced by the Village Hall array



Village Hall Rooftop Solar Array (J. Bartol)

ARCHITECTURAL GUIDELINES

1. Materiality.

- Primary street facade material must be masonry.
- Masonry may not be larger than the standard-size brick (8" L x 3 5/8" W x 2 1/4" H)
- Brick selection must be of a warm hue;
 - Red, Tan, Cream, Brown
- Non-public facades may be of fiber-cement panel, or like materials.
- Any pitched, or mansard roof material must be of high quality.
- Wood or metal may be used around storefronts on the ground floor.

2. Ornamentation.

- Flat-roofed buildings must include a cornice element.
- Decorative brick cornices with significant protrusions from the facade are encouraged.
- Extruded painted metal cornices may be used.
 - Masonry detailing is encouraged, including;
 - Varied bonding methods; Flemish, English, Common, etc.
 - Brick quoining on building corners.
 - Brick window header details.
 - Brick pilasters.
 - Brick sills.

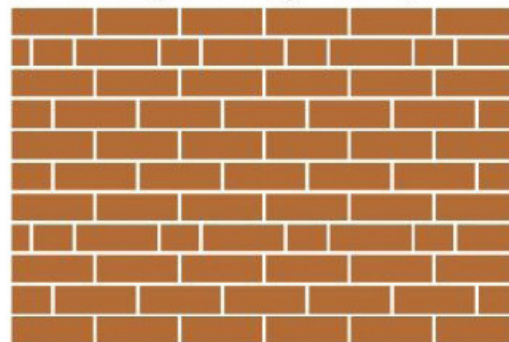
LANDSCAPING

1. Wherever possible, landscaping is to be employed as a buffer.

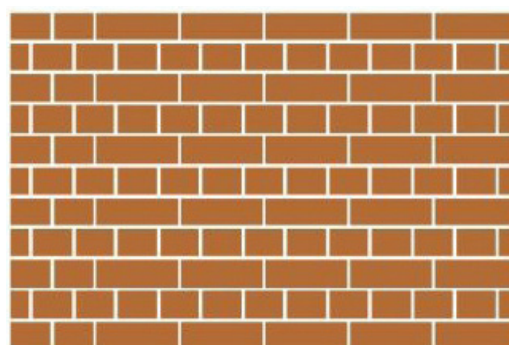
- Street trees are to be used wherever possible.
- Planters.
- Landscape strips along secondary street facades where buildings are not mandated to be built to property lines.

2. Public plazas or other pedestrian areas apart from sidewalks should be paved with permeable pavers or include other stormwater mitigation methods including bioswales and rain gardens

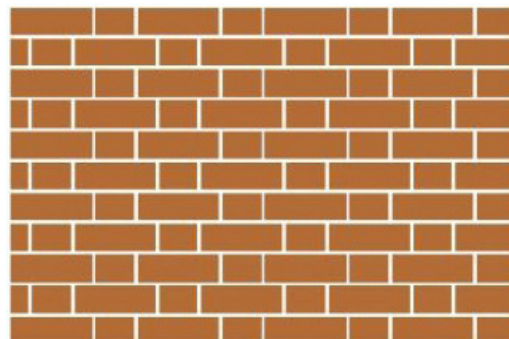
Common Bond (Full headers every 6th course)



English Cross or Dutch Bond



Flemish Bond



Masonry Bonds

Including sections of unique bonding methods adds texture to the facade and aligns with the historic character of the district.



Bioswale

Rather than landscaping the road verge with turf, implementing bioswales both add more diverse plantlife and help mitigate stormwater run-off.

ARCHITECTURAL GUIDELINES

HISTORIC BUILDING: ELEVATION STUDY

PURPOSE

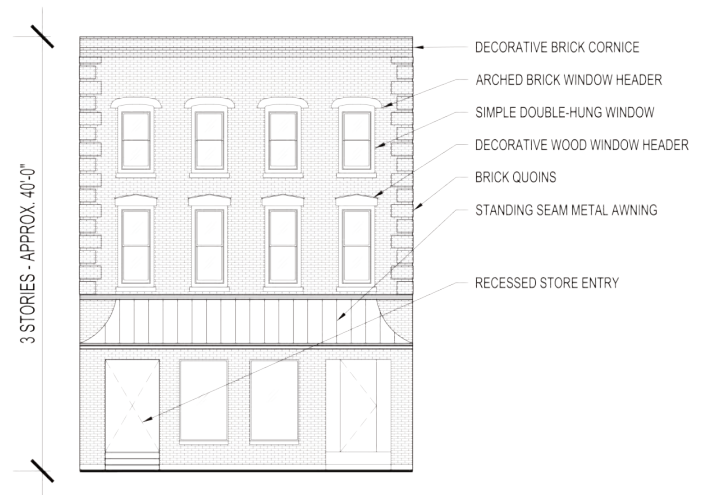
Despite the village's history of loss, Downtown Lancaster still retains a strong supply of historic buildings. In order to maintain the character of the village center new development must draw on inspiration from its neighbors. Façade studies inform the guidelines later laid out in this document in regard to materiality, ornamentation, and façade layout.

FINDINGS

1. Buildings do not exceed three stories, with the exception of the Opera House.
2. Brick (red) is standard.
3. Windows are vertical, punched, and embellished with ornamented headers.
4. Cornices are employed either with brickwork or wood.
5. Historic details remain intact such as:
 - Brick quoining
 - Decorative moulding



4 Main Street Elevation (J. Bartol)



5 Main Street Elevation (J. Bartol)

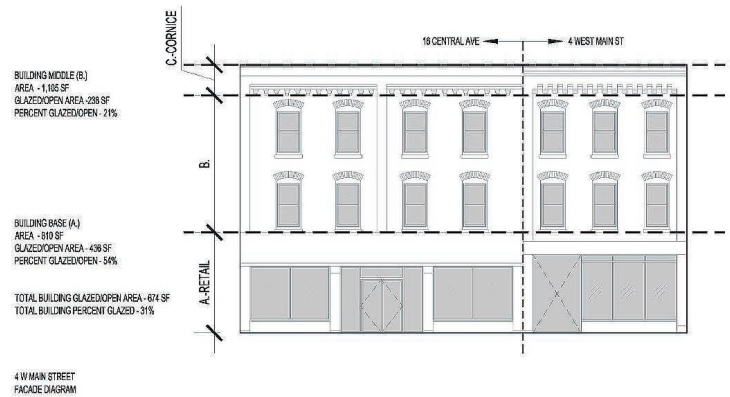


1 Main Street Elevation (J. Bartol)

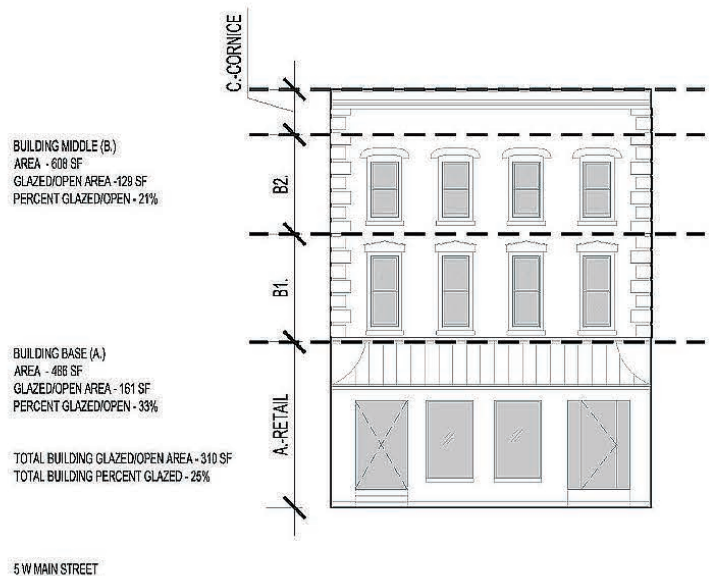
ARCHITECTURAL GUIDELINES

FINDINGS

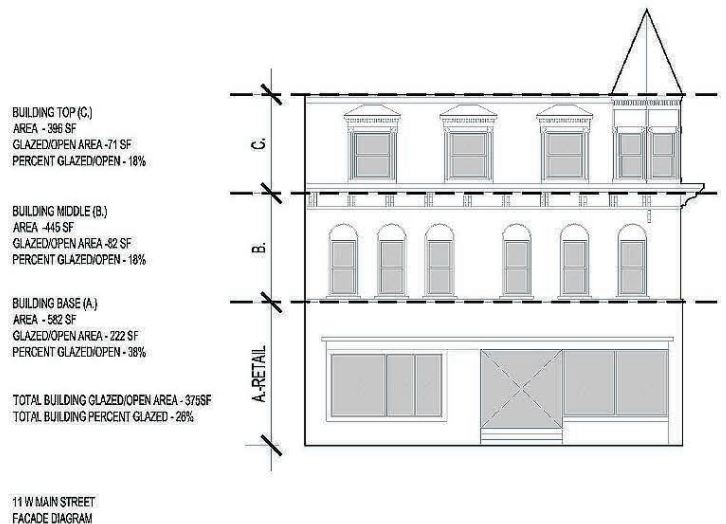
1. Ground floor storefronts have been altered from their historic forms/styles.
2. Buildings have a distinct base and middle and are topped by either a cornice or a unique third section.
3. Upper floors are, on average, comprised of 20% openings.
4. Ground floors are, on average, comprised of 41% openings. (As high as 54%).



4 Main Street Elevation (J. Bartol)



5 Main Street Elevation (J. Bartol)



1 Main Street Elevation (J. Bartol)

THE HUB : URBAN DESIGN GUIDE-

DOWNTOWN LANCASTER BUILDING HEIGHT MAP

Buildings downtown primarily consist of 2-3 stories, especially in the historic district, where over 70% of buildings are just 2 stories. The main outlier in height is the prominent Lancaster Opera House, with the main mass of the building being 3 stories and the central tower at a total of 5 stories. Due to the existing conditions, the majority of parcels downtown are allowed a maximum height of 40' or 3 stories. Parcels within the Central Avenue Historic District are limited to 30' or 2 stories, as are parcels adjacent to the eastern boundaries of downtown that abut the residential areas. Prominent corners are permitted building heights of 50' or 4 stories. These are intended as "Gateway" developments, marking the entries to Downtown.

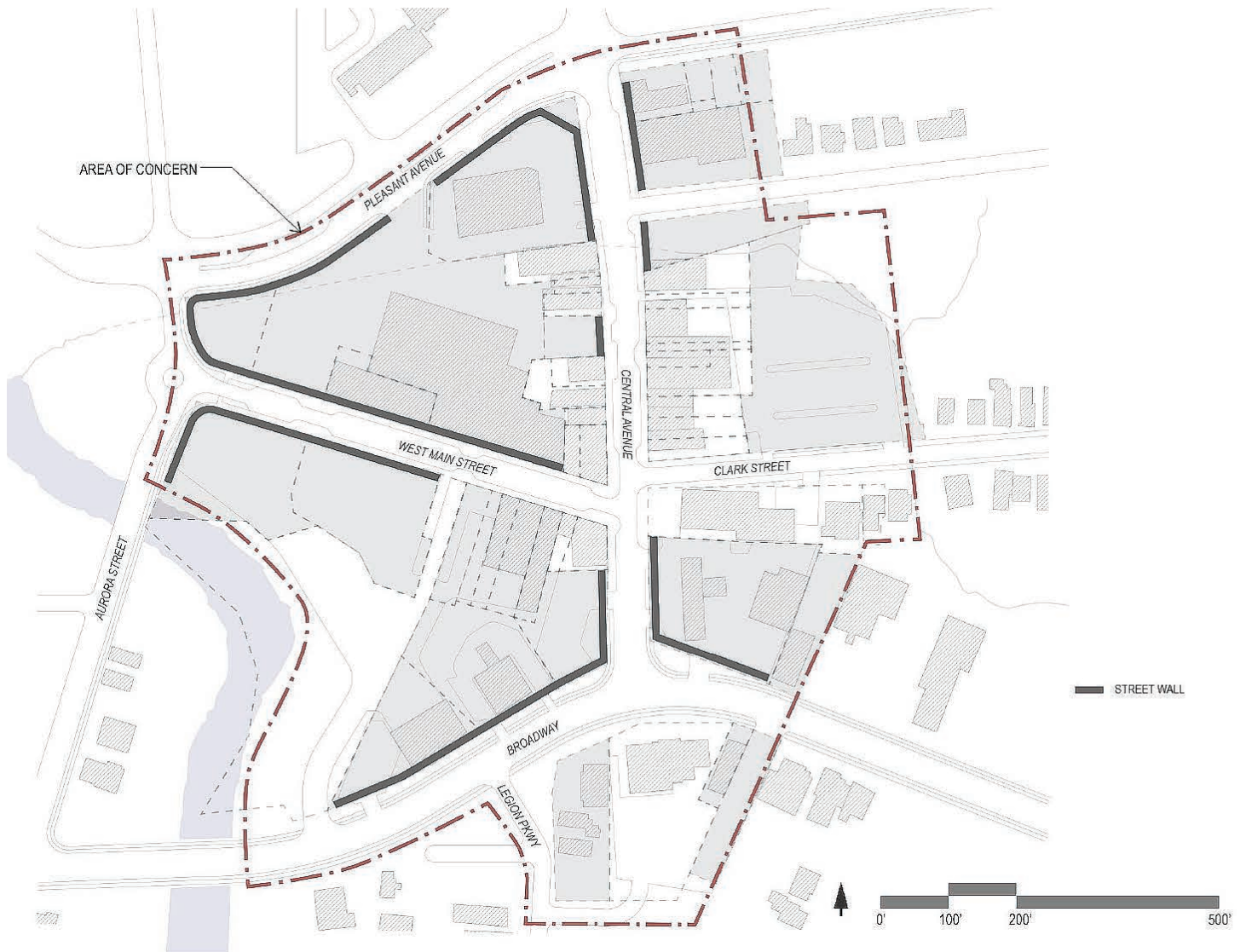


Proposed Height Map (J. Bartol)

THE HUB : URBAN DESIGN GUIDE-

DOWNTOWN LANCASTER BUILD-TO LINES MAP

Buildings fronting Aurora Street, West Main Street, Central Avenue, Pleasant Avenue, and the north side of Broadway are to build to the property line. The properties fronting Pleasant Avenue to the east of Central Ave, as well as the properties fronting Clark Street, are not mandated to build to the property line and are instead encouraged to align their facades with adjacent buildings.



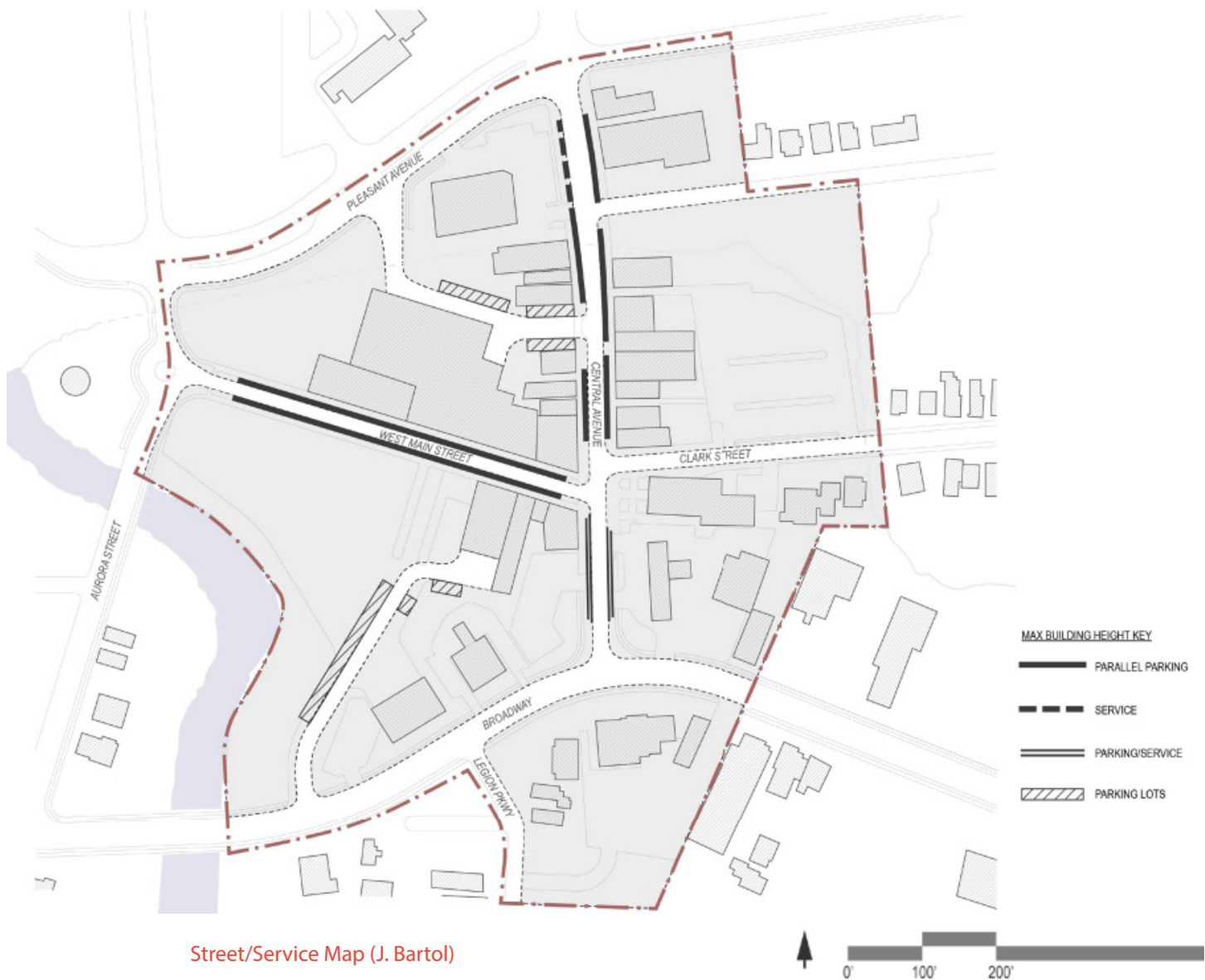
Build-To Lines Map (J. Bartol)

THE HUB : URBAN DESIGN GUIDE-

DOWNTOWN LANCASTER STREET/SERVICE PARKING MAP

Street parking largely remains as is along Central Ave and W Main Street. A street in the southwestern block of downtown is reconfigured to allow better access to the future Cayuga Creek Park, as well as to provide parking for said park. A street is added to the northwest block of downtown to improve access to the grocery store and future development. Some surface parking is provided there as well.

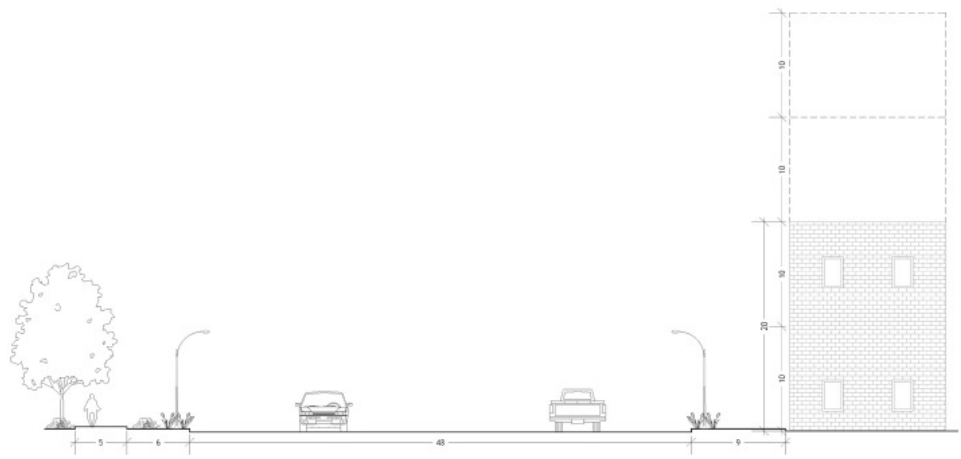
Street parking at the north and south of Central Ave is changed to service areas or parking/service areas for future development.



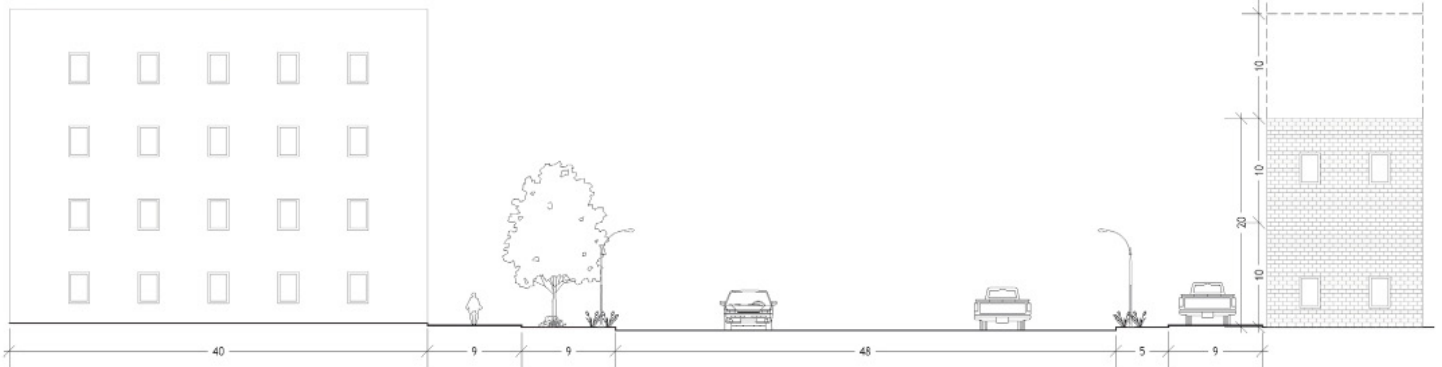
THE HUB : URBAN DESIGN GUIDE-

STREET SECTION DIAGRAMS

The section display the proposed allowable building heights and the affects on the pedestrian experience downtown. While any development, no matter the height, contributes to completing the street wall, taller buildings create a stronger sense of enclosure and can create more distinct urban spaces.



Proposed Height Section Diagrams (P. Balaji)



Proposed Height Section Diagrams (P. Balaji)

THE STRIP : URBAN DESIGN GUIDE-

- Creating a walking path cutting in between M&T Bank & Lancaster Department of Public Works that allows access to walking along the berm of Cayuga Creek
- A pedestrian connector passes between north through buildings, providing shortcuts through long blocks and sometimes connecting rear parking areas with frontages.
- Walking path signage towards Cayuga Creek will be located within the greenspace between M&T Bank & Lancaster Department of Public Works
- A public information sign is any sign that provides information on the use of the facility, such as directional signs, trailhead entry signs, and information kiosks.
- 3 lane road
- Accommodations for pedestrians, cyclists, motorists, transit riders, and the mobility-impaired.
- Mixed-Use Street
- Lane. A narrow, slow-movement thoroughfare, typically containing one travel lane.
- Side parking
- How the anticipated travel demand for the project will be met on-site or off-site, including:
 - Number of on-street vehicle parking spaces, off-street vehicle parking
 - A buffered bike lane
 - The strategies will reduce single-occupancy vehicle trips, reduce vehicle miles traveled by site users, and promote transportation alternatives such as walking, cycling, ridesharing, and transit.
 - A buffer zone located between the bike lane and the sidewalk
 - The parking lot will still exist but it will relocate toward the back of the M&T Bank
 - A parking lot must provide pedestrian facilities in accordance with Section 8.3.3.B.
 - The M&T Bank will be moved closer to the sidewalk (Built to the edge) to improve the building setback
 - The M&T parking lot will have an exit and entrance on one side of the bank
 - Pedestrian facilities must be provided connecting main entrance to parking, adjacent public rights-of-way and transit stops and stations, and all uses on a site that allow for public access
 - Where sidewalks cross driveways, the throughway zone must remain level, with no change in cross-slope. The appearance of the throughway zone, such as scoring pattern or special paving, must be maintained across
 - There will be a walkway that is accessible from the sidewalk to M&T Bank
 - Pedestrian facilities must consist of accessible, easily discernible, and ADA-compliant walkways. The pedestrian facilities must be paved with a fixed, firm, and non-slip material.
 - The proposed gateway signage will be located at the western edge of the Village of Lancaster over Broadway
 - The sign has a unique location or contains singular physical characteristic that make it an established or familiar visual feature within the City (Heritage signs)
 - The wall sign will remain the same for M&T Bank
 - A maximum of one canopy sign is permitted per establishment per right-of-way frontage.
 - The facade of M&T Bank will be fully transparent allowing sunlight to shine through, better working environment with views of the outside, and allowing customers or passers by to see what is happening and what type of facility this is.

URBAN DESIGN GUIDELINES

- All principal buildings must be oriented to the thoroughfare they face, with all front and corner side facades positioned parallel or radial to right-of-way lines.
- The ground floor of any facade that exists along a PPF must have a minimum transparency of 70%.
- Exterior Facade Restrictions. Where exterior facade materials are restricted on front and corner side facades, these restrictions also apply to other facades in the following circumstances:
 - The building height of the bank will remain 1-2 stories
 - Irrespective of the provisions in Sections 3.2.2-3.2.12, a principal building may be erected to a height of up to six stories and 76 feet on any parcel along the strip
 - A vehicular right of way for the entrance and exit of the building
 - A vehicular drive located to the rear of lots provides access to service areas, parking, or accessory structures, and contains utility easements.

THE KNUCKLE : URBAN DESIGN

SITE R.O.W. SECTIONS

The comparative analysis between the existing and proposed section at aurora street defines the use of the retaining wall against the creek which is built to prevent from overflow could be used as creekside pedestrian walkway and Trailway, The proposed townhouses and mixed-use buildings also help in defining the space. The heights shown in the section of the townhouse as well as the mixed-use building complement the surrounding heights of the structure acting as a catalyst - the missing middle between The Knuckle and The Hub. Along with that, the proposed section believes in widening the pedestrian walkway from 9'6" to 12' increasing the right of way from 56' to 58'



Existing Aurora street section (V. Chavan)



Proposed Aurora street section (V. Chavan)



Proposed Aurora street section (V. Chavan)

Project proposals were chosen along US Highway 20 - Broadway at mirroring sites within the three study areas of the village. Each project site represents an area within the village that is a sub-optimal use of space in the scope of urban form. Each site will present a development project that will tackle key challenges in urban form and design. While each group tasked with proposing these projects took differing approaches to solving these challenges, common themes arise in their methods. Each group has identified the movement of pedestrians, and access to commercial space as key principles in the design choices. The goal of these projects is not to suggest that these are the only ways that Lancaster can improve, but rather that these are the design principles that should be placed on future projects.



7.1 - THE HUB A: PROPOSAL FOR MIXED USE DEVELOPMENT

A MIXED-USE DEVELOPMENT PROPOSAL

SETTING A FRAMEWORK

After analyzing the existing conditions of the Village of Lancaster, new guidelines were constructed for how the village can improve the urban landscape. Following the new urban design guidelines, a proposal was made. The proposal for this stage follows the guidelines for.

- Building Height
- Materiality
- Build to line
- Parking locations
- Ornamentation
- Landscaping
- Programming

These guidelines created a structured design that was shaped by the strict guidelines, allowing for the proposed structure to be set according to the location and market conditions.



Proposed Building Within Downtown (M. Marun)

7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL

GAS STATION SITE

The proposed development was set on the corner of Central Ave and Broadway, where the existing Sunocogas station currently resides. The site features a large amount of unused paved space on a corner that would be a highly coveted site for new development. Adjacent to the gas station stands Lancaster's Broadway Deli and Opticians office. These programs were slotted as a desired program for the site, but with the undesirable gas station in this location, the proposal was slotted to take over the entire parcel of land and propose a new development that would better suit the surrounding Village of Lancaster.

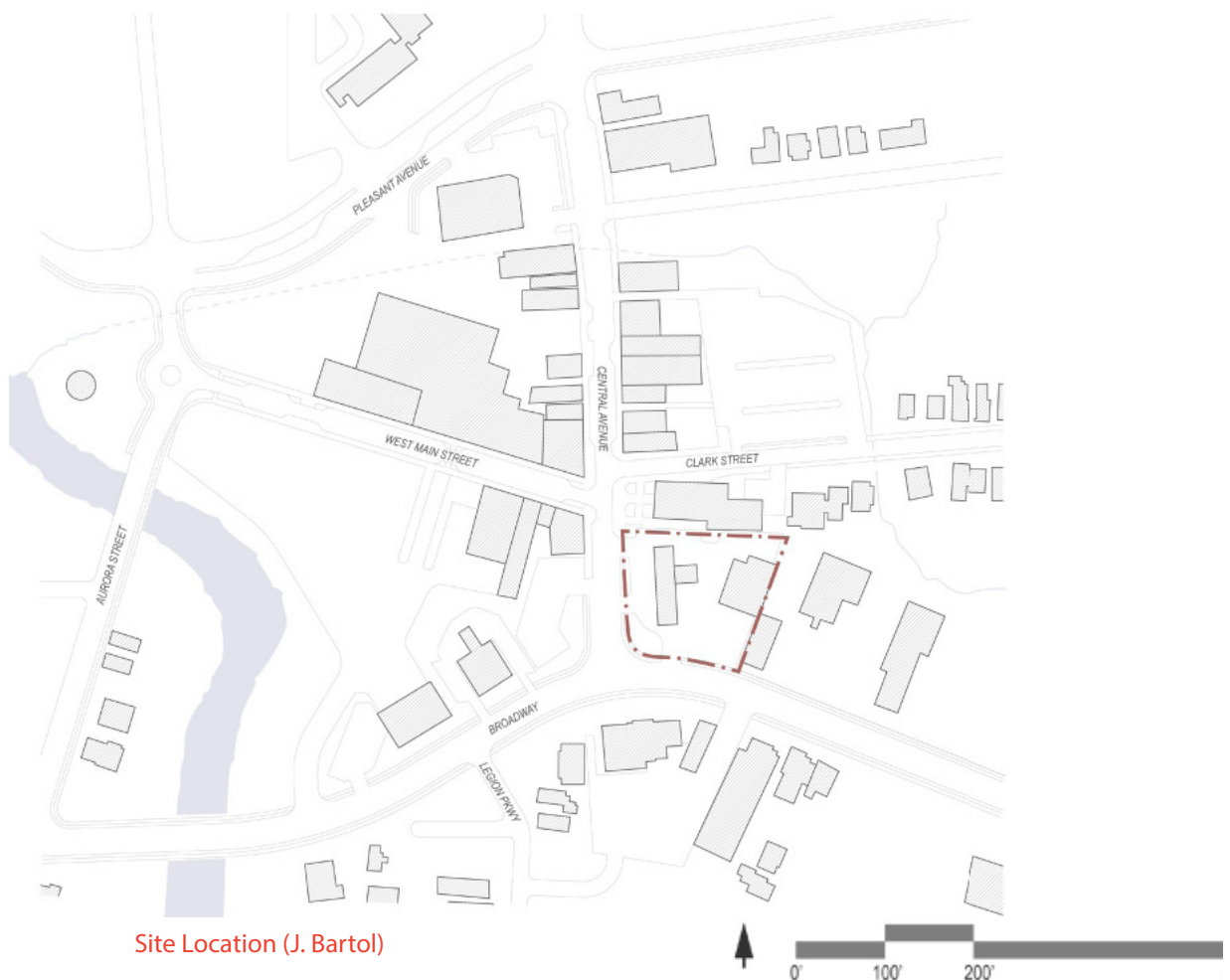
North of the site of new development stands the Lancaster Town Opera House as well as the Lancaster Town Clerk. These buildings stand as a historical landmark for the Village of Lancaster and cannot be touched but should instead be emphasized and strengthened by the new development's placement in this area.

With these points in consideration, the new development must -

- Create new leasable spaces after replacing existing programs
- Incorporate new housing units for new development
- Match the existing characteristics of the surrounding buildings
- Improve the corner site into a location much more attractive to passing pedestrians

The new development must not be -

- Overshadow historical landmarks of the Village of Lancaster
- Create under-designed or non-pedestrian-friendly paved space



7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL

MASSING

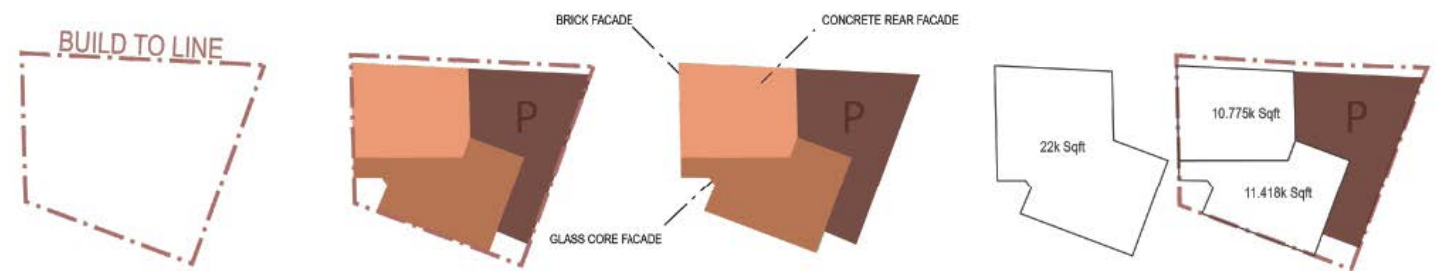
The proposal's form took shape by following the urban design guidelines made to control.

- Height
- Build to Line
- Parking Locations

Using these Guidelines, the building's form becomes a three-tiered structure. The three tiers of the proposal highlight different areas of the design that were reconsidered, aligning with our lists of dos and don'ts.

The mixed-use development for the proposal will appease the market demand for more leasable retail space on the ground floor while allowing for the projected absorbable square footage of residential housing to be represented in a range of units. While incorporating enough square footage of retail and residential units' consideration still needed to be paid to the historical opera house. This created a two-tiered design that would frame the view of the adjacent opera house better and not overshadow the existing buildings. The two proposed masses were.

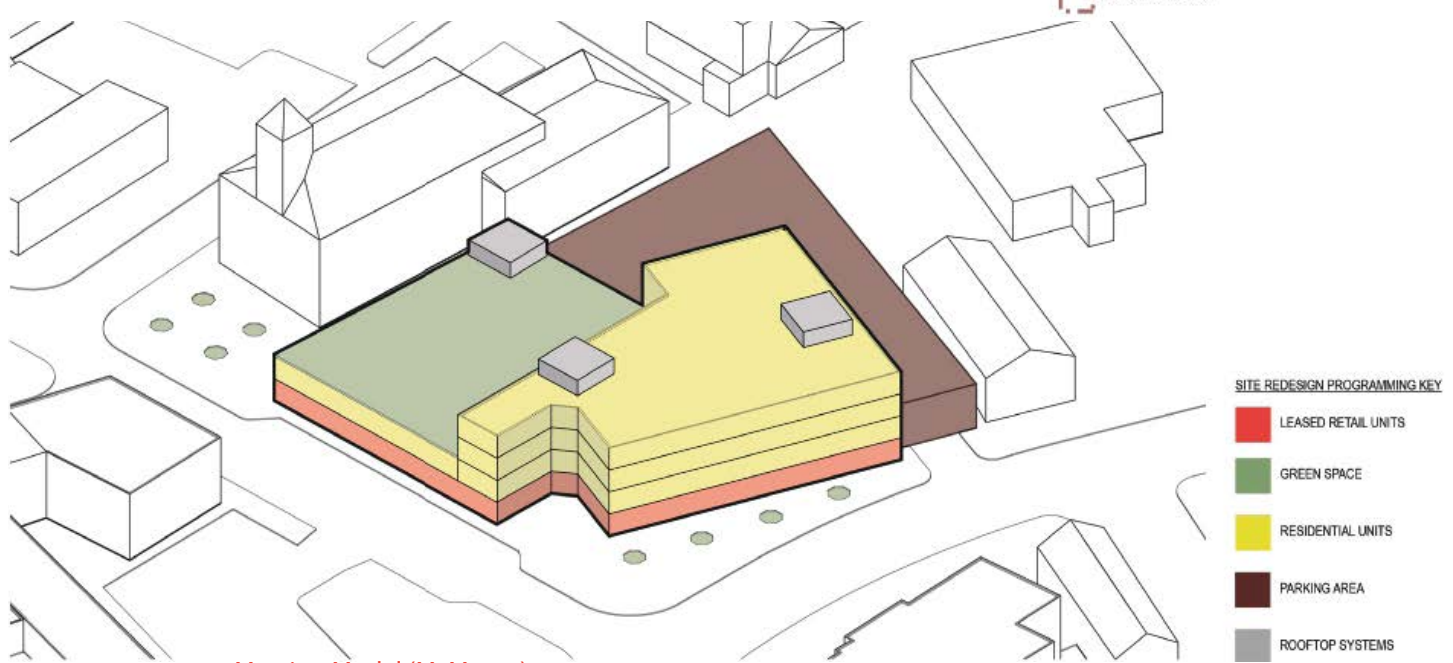
- A 20' Green roof Mixed-Use Development (21,550 SF)
- A 40' Corner Mixed-Use Development (45,670 SF)



SITE REDESIGN MASSING (PROPOSAL 01) KEY

- MIXED USE DEVELOPMENT
- GREEN ROOF - MIXED USE DEVELOPMENT
- PARKING AREA
- BUILD TO BOUNDARY

Massing Model (M. Marun)



Massing Model (M. Marun)

7.1 - THE HUB A

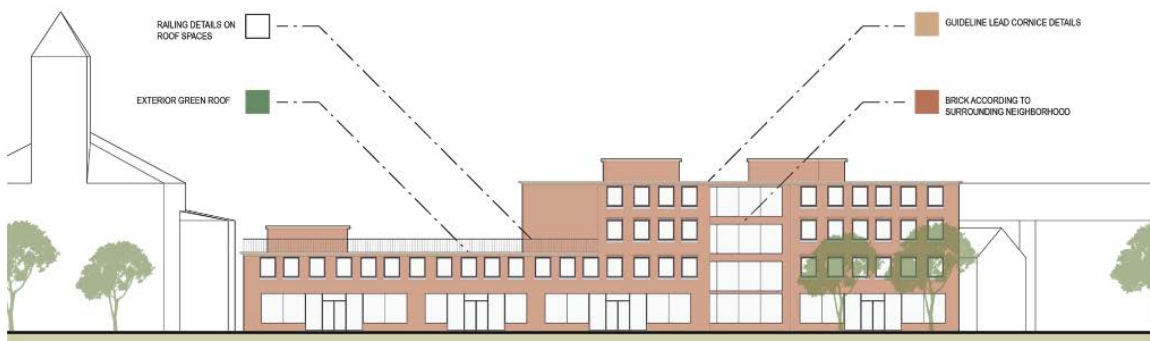
A MIXED-USE DEVELOPMENT PROPOSAL

DESIGN

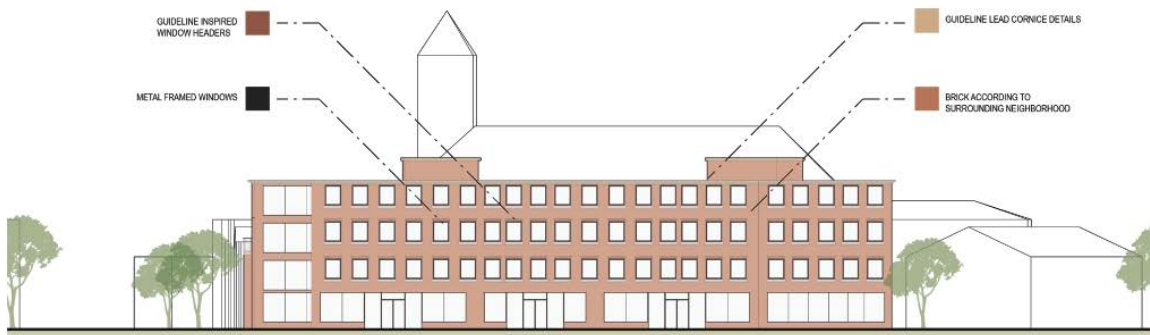
For the new proposal to match the historical detailing of the surrounding buildings in Lancaster, attention was paid to the form of the massing as well as the building's envelope. The building's envelope will match detailing elements such as:

- 81% Glazing Store frontage on the ground floor
Allowing for greater pedestrian visibility into store fronts
- Creating a permeable ground floor condition. (ACCORDING TO GUIDELINES)
- Windows to proposed units with historically inspired window headers and sills

- Matching to surrounding areas glazing conditions
- Creating a synchronization between the pedestrian landscape. (ACCORDING TO GUIDELINES)
- Red Brickwork to match surrounding buildings with typical masonry design. (ACCORDING TO GUIDELINES)
- Cornices at the roof level to accentuate the roofline and add to the historical look
- Matching to surrounding buildings and nodding to the existing buildings. (ACCORDING TO GUIDELINES)



Central Ave Elevation (M. Marun)



Broadway Elevation (M. Marun)

7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL



Aerial Perspective (Master Plan Proposed Buildings in Yellow) (M. Marun)

GREEN SPACE

Greenspace on site was considered for the new development to decrease the amount of built space. In the adjacent opera house building, the Town has incorporated four tree plantersto improve the streetscape for pedestrians and break up paved space. The same strategy was considered for the corner space near our site to match the characteristics of the adjacent site and create a seamless pedestrian experience. These spaces should mesh by using

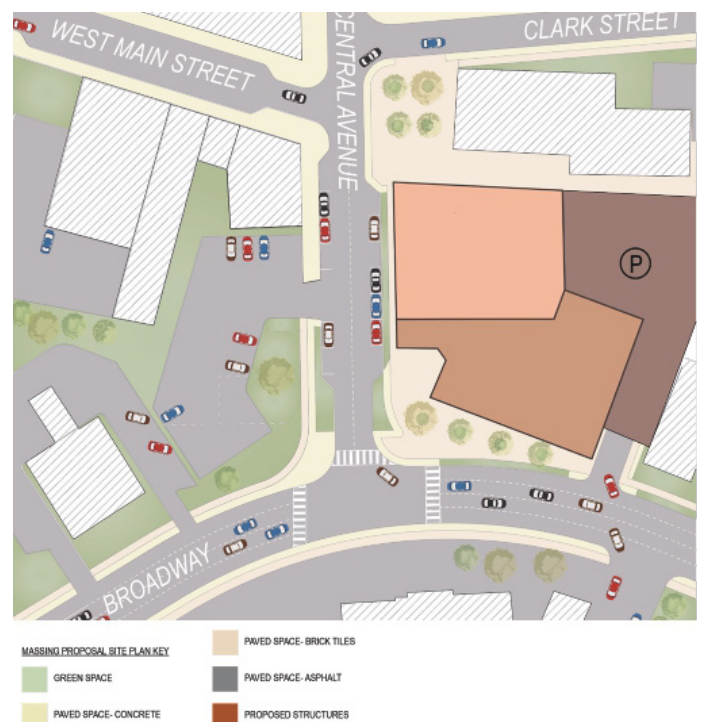
- Matching street pavement
- Matching Tree Planters
- Matching existing materials for bump-outs

To enhance the residents living conditions, a rooftop greenspace was proposed to increase the amount of greenspace square footage on site. By increasing greenspace on the roof top, it creates a new intimate outdoor space for people to inhabit that overlooks the opera house and promotes outdoor activities.

- Spaces incorporated would feature.
- Shared recreational space
- Resident run gardens Shared benches and tables

PARKING

Parking for the new development is designed to be affordable while maximizing the allotted space on site. This proposed design was identified by using a podium-style parking structure. This podium parking will allow for low costs on materials and a greater amount of parking spaces to support the new development and reduce the amount of paved space needed on site.



Site Plan (M. Marun)

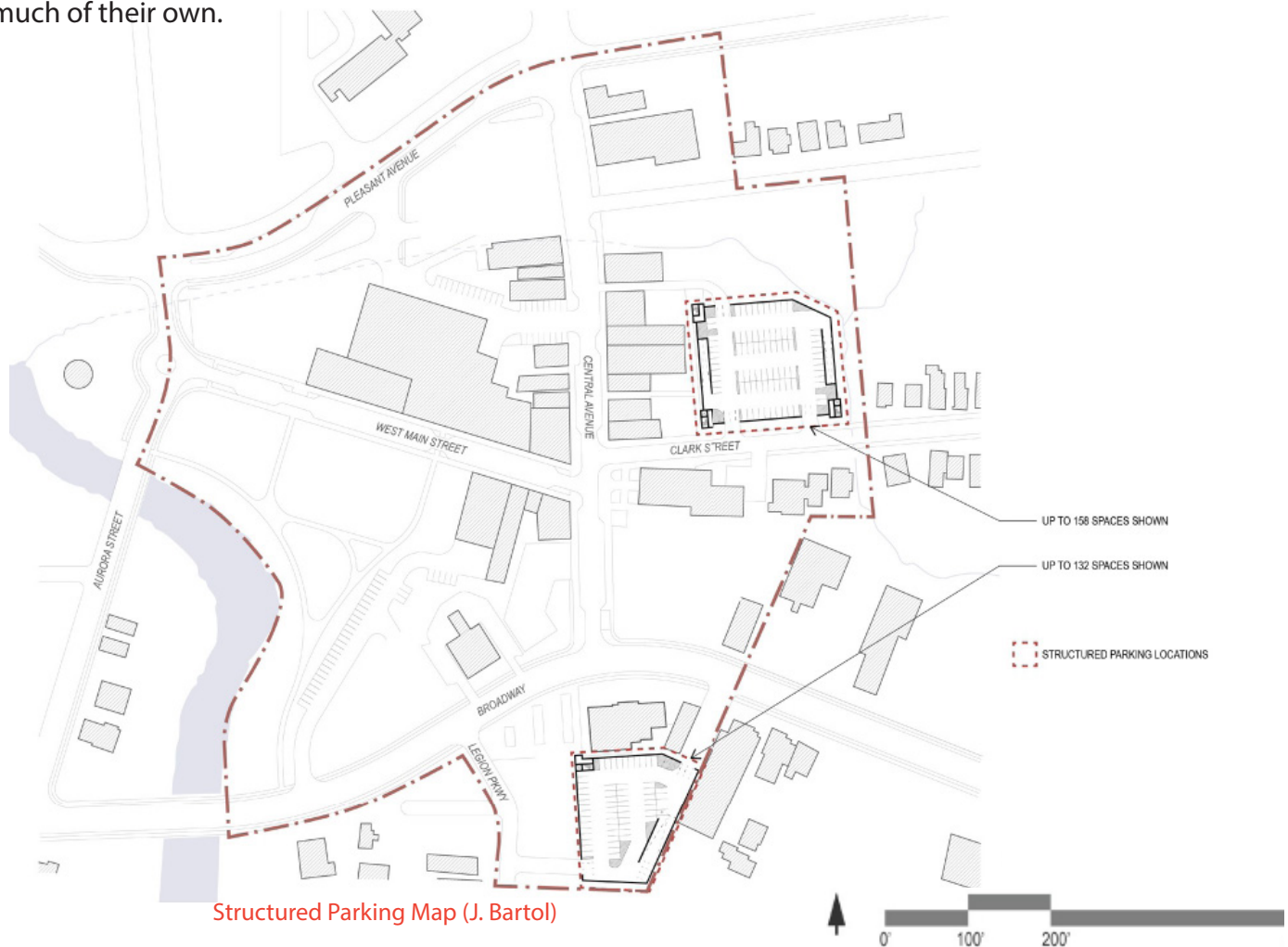
7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL

STRUCTURED PARKING MAP

Surface parking is highly discouraged, and whenever possible, structured parking should be constructed. Two areas, in particular, are recommended for structured parking.

1. Along Clark Street behind the historic buildings on Central Ave. With 2 stories, this garage could hold an estimated 158 spaces.
2. Behind Village Hall. With 2 stories, this garage could hold an estimated 152 spaces.
3. Structured parking can potentially be large enough for both visitors and residents of adjacent new development. If given the opportunity to rent spaces from village-owned garages, developers would be able to save on construction costs by not needing to provide much of their own.



7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL

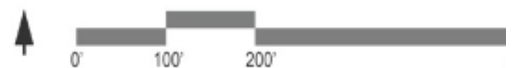
GREENERY MAP

Green space is provided wherever possible.

An opportunity exists around Plum Bottom Creek for another public park. The land immediately west of the Village Hall also provides an opportunity for a park. A public park here would provide a pleasant welcome to the downtown as well as maintain that space as open to protect views of Village Hall to visitors traveling along Broadway from the west.



Green Infrastructure Map (P. Balaji)



7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL

FINAL PLAN

The final plan for Lancaster’s Downtown district is dramatically different than first outlined in this document. Twelve new buildings or implementations are proposed as well as two new village parks (not including the under-construction Cayuga Creek Park). Some streets within the two west blocks of Downtown are reconfigured to allow better access to parking garages, surface parking, Cayuga Creek Park, and the grocery store. These access roads also ensure garage entries are not located off of main roads.

A. Mixed-Use, Apartment/Retail

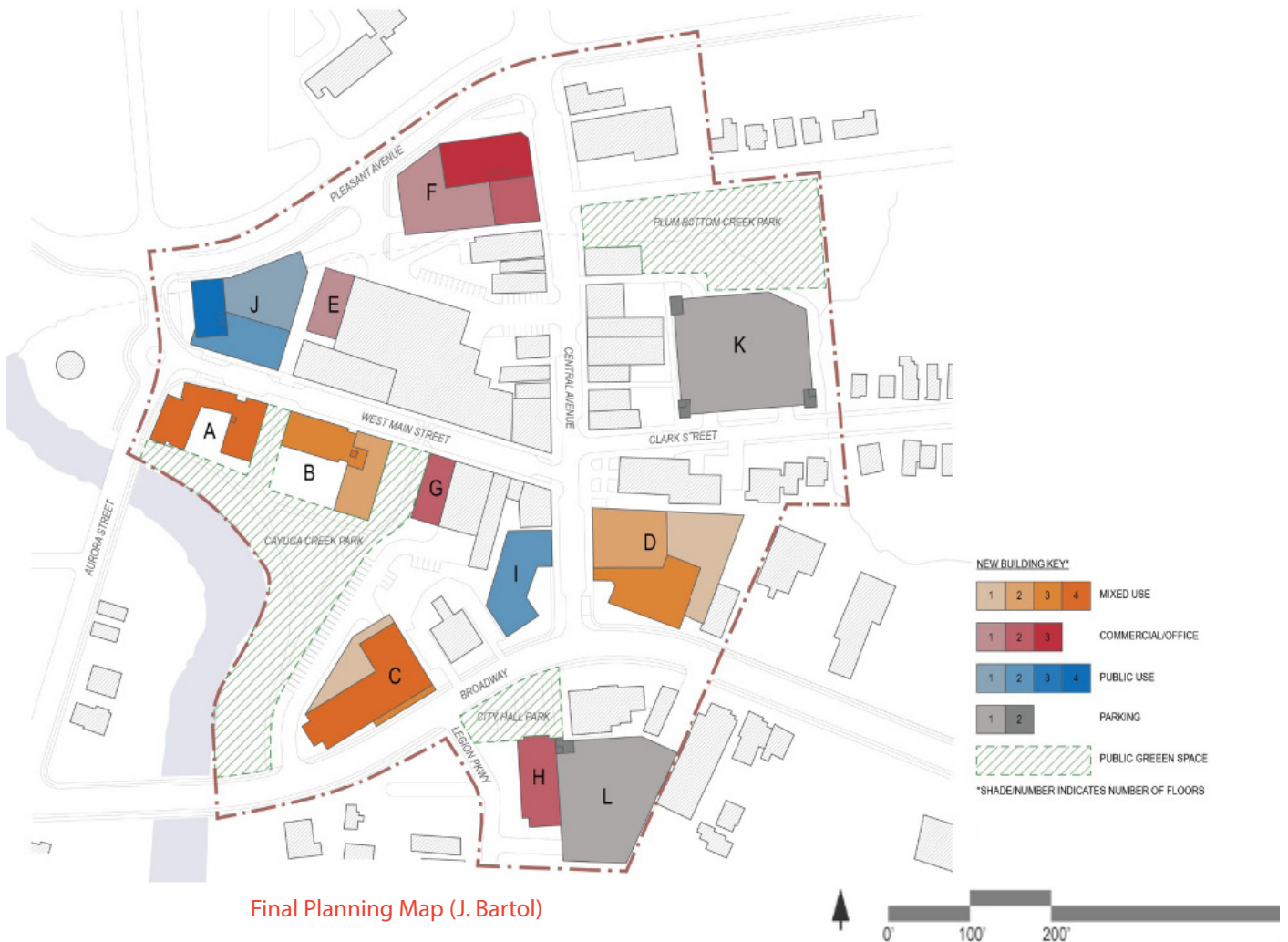
1. Up to 33 units.
2. 4 stories.
3. Envisioned having a restaurant on the ground floor to have a terrace overlooking the creek.

B. Mixed-Use, Apartment/Retail

1. Up to 20 units.
2. 2-3 stories.
3. Envisioned having a restaurant on the ground floor to have a terrace overlooking the creek.

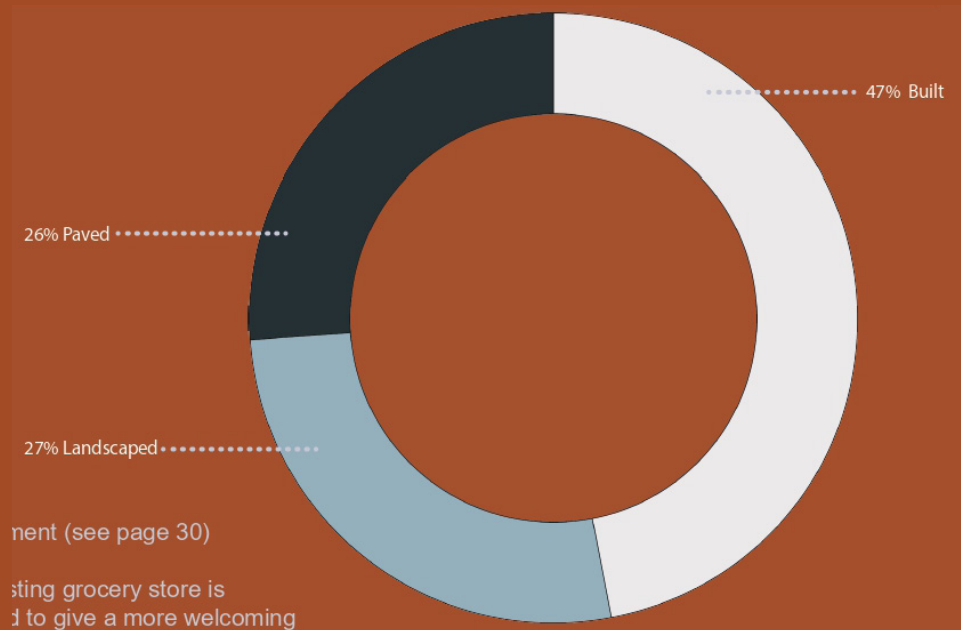
C. Mixed-Use, Apartment/Retail

1. Up to 40 units.
2. 4 stories.
3. Structured parking (ground floor + basement). Structured parking would include spaces for developments A and B to rent.



7.1 - THE HUB A

A MIXED-USE DEVELOPMENT PROPOSAL



- D. Proposed new development.
- E. The entrance to the existing grocery store is expanded and refreshed to give a more welcoming and modern look.
- F. Proposed Health Clinic
 1. 2-3 stories.
 2. Structured parking.
 3. This could include a pharmacy on the ground floor to replace the existing CVS and serve the nearby nursing home.
- G. Commercial Building
 1. 2 stories.
- H. Office Building
 1. 2 stories.
 2. Entry off of the new village hall park.
 3. Could also contain a cafe on the ground floor to serve municipal employees.
 4. Direct connection to the adjacent parking structure.
- I. Proposed new Village Hall
- J. Proposed Community Center + Business Incubator
 1. 2-4 stories.
 2. Structured parking. This would include parking for shoppers at the adjacent grocery store.
 3. A small public plaza is proposed at the west end of the property.
- K. Proposed structured parking.
- L. Proposed structured parking.

BUILDING STOCK

41 Buildings

5 Mixed-Use

26 Commercial

6 Institutional

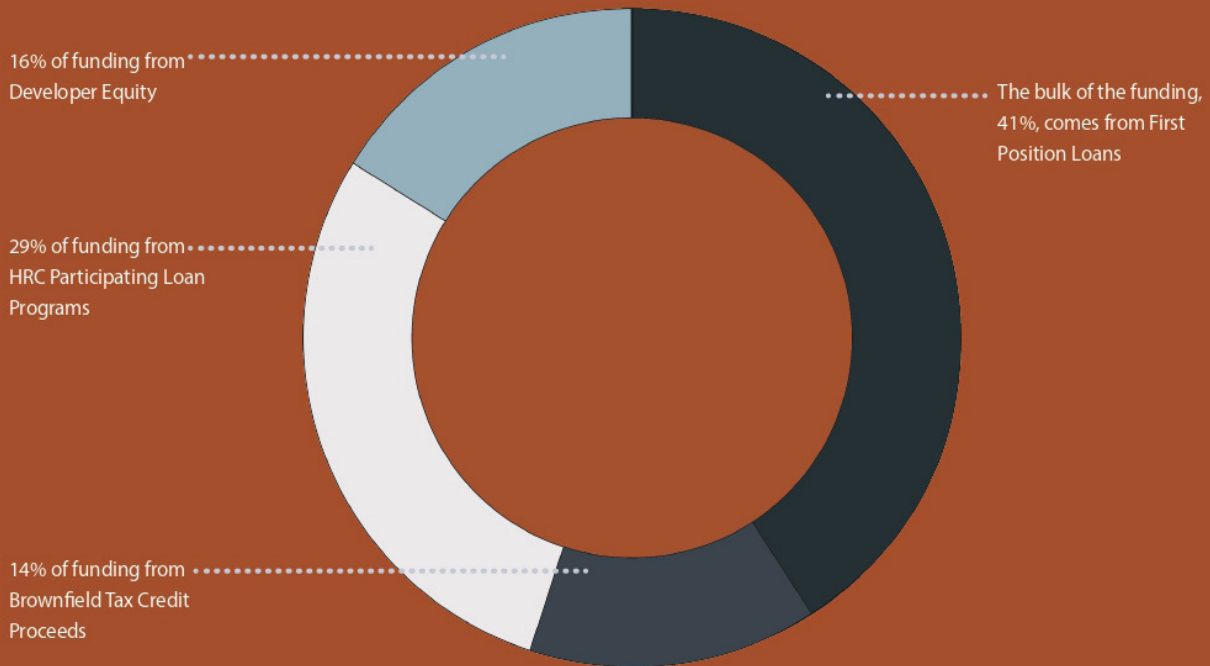
2 Single-Family Housing

2 Parking Structures

111 Apartment Units

THE HUB A : FINANCE

A MIXED-USE DEVELOPMENT PROPOSAL



PROPOSED DEVELOPMENT

Our proposed development will be where the Sunoco Gas Station is located at Central Ave and Broadway. The new construction mixed-use development is 67,000 square feet and consists of 9 retail units on the first floor and 55 apartments. From an underwriting perspective, the apartments are being leased below 80% AMI, and the commercial space is leased out at \$15/SF annually. Both of these rents are at or below competitive market demands. Our development also includes seven three-bedroom apartments which the village currently does not have.

Our permanent sources consist of the following:

- First Position Loan: \$7,530,983
- Brownfield Tax Credit Proceeds (after tax): \$2,462,233 HCR
- Participation Loan Program (PLP): \$5,225,000
- Developer Equity: \$2,959,668

67,000 SF

5 Retail Units

\$15/sf Retail Rent

55 Apartments

\$1,000 Rent - Studio

\$1,200 Rent - 1 bdrm

\$1,550 Rent - 2 bdrm

\$1,800 Rent - 3 bdrm

This capital stack assumes that the former gas station will be eligible for the Brownfield Remediation Program. Our project also utilizes an affordable housing gas-of-right loan program through HCR for developments providing housing for households making below 80% AMI.

Calculations by (E. Finegan)

THE HUB A : FINANCE

A MIXED-USE DEVELOPMENT PROPOSAL

8.1 - THE HUB (A)

CONSTRUCTION COSTS

The construction cost of the development are estimated to be \$200/SF with an additional 12% for soft cost. Another larger line item expense is the 50 unit parking garage that will cost \$250,000. The Brownfield Tax Credits have an applicable percentage on tangible property costs of 20%. This is calculated by a 10% base and an extra 5% for affordable housing and being in an EnZone. The operational expenses are directly from CPC. The income and expenses are modeled to have an annual growth of 3%.

This development currently utilizes a tax abatement program (which we have not confirmed the Village of Lancaster has opted in for) RPTL 485-a. This tax abatement program is for mixed-use developments with affordable housing components. Tax abatements are essential to understand the feasibility of these development projects. Upon completion, the new development will receive a reassessed value from the city and county post-completion. This can make the development not make enough money to be profitable or even pay the debt service. Once the PILOT program runs out, the full assessment will occur.

The current projections of the development reflect a 10 year IRR of 15.55% with an equity multiple of 3.20. A development like this one could be a catalyst for the Village and infectious to the areas surrounding it. By promoting walkability, density, and increased retail, our proposed development would positively impact the community.



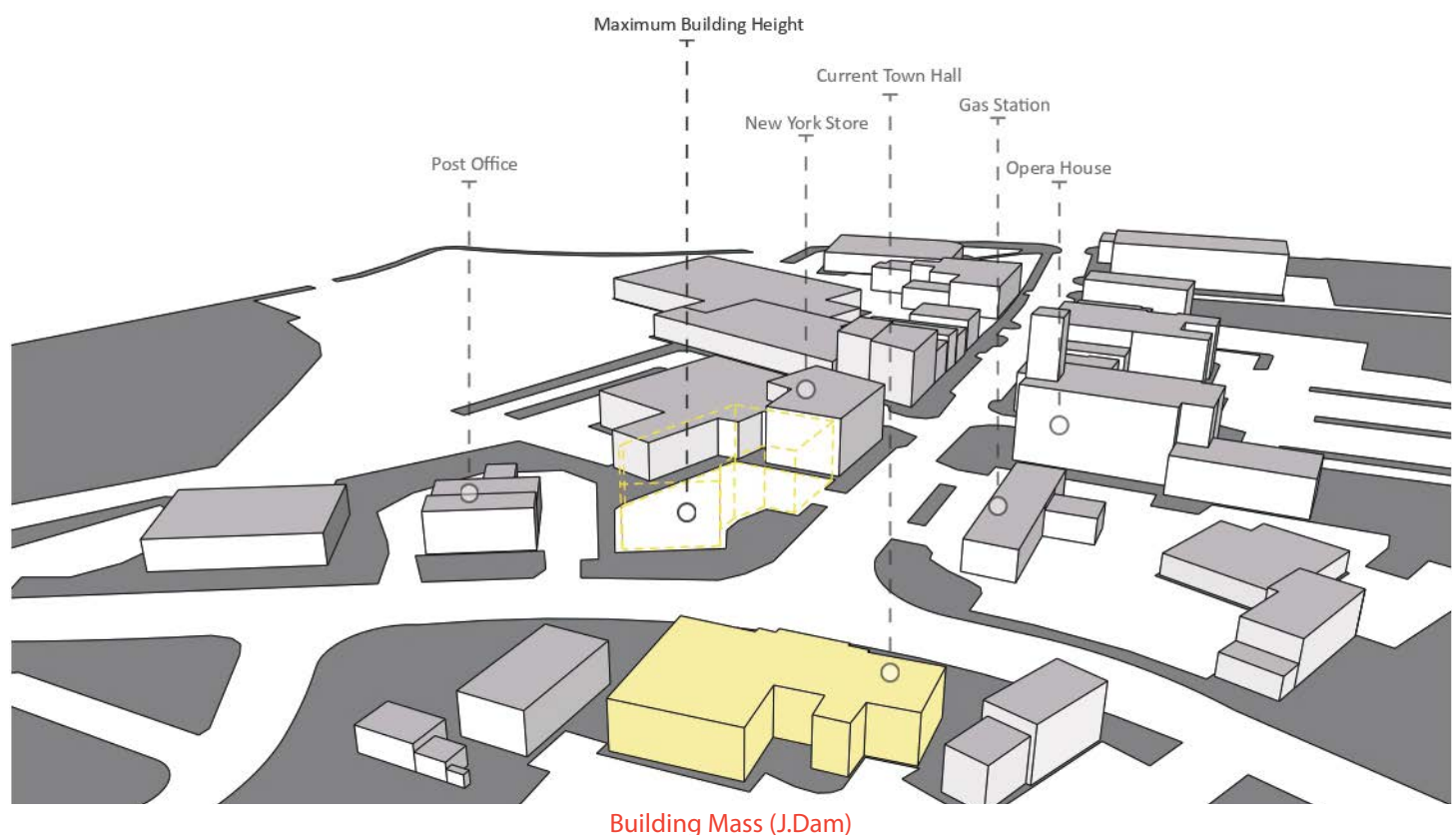
Aerial Perspective (Master Plan Proposed Buildings in Yellow) (M. Marun)

Calculations by (E. Finegan)

7.2 - THE HUB B: PROPOSAL FOR A NEW TOWN HALL

An identified missing piece of the Hub north of Broadway is a unified civic anchor point. The proposal for a new municipal center, positioned in the parking lot on the northwest corner of Broadway and Central Avenue, will serve as a connecting point for highly walkable areas on the site from development happening on the east shore of Cayuga Creek, to the vital areas of Central Avenue and West Main Street. The municipal center is designed drawing inspiration from global civic center projects, and utilized a new building material in Timber. While this differentiates from the brickwork common in the Hub, it can be used to signify a Lancaster of the future, using sustainable and lasting materials to create a lasting civic identity.

The proposal will also increase the visibility of the town hall, which is partially hidden on the south side of Broadway, with a greater visual emphasis placed on the fire-hall. The proposed site would call for the maintaining of the fire hall, with existing municipal space being re-purposed for meeting space or community space. This will allow for a clear area of communal congregation for the community, limiting the split concentration of communal space between the existing municipal building and the Opera House



7.2 - THE HUB B

PROPOSAL FOR A NEW TOWN HALL

CONNECTIVITY

Showing the interconnectedness of the new proposal, particularly for pedestrians, this site plan calls out to the ease of movement through the area. It also shows how accessible the new proposal is to the surrounding context, and improves the area.



Existing Render (J.Dam)



Proposed Render (J.Dam)



Perspective (J.Dam)

THE HUB B: FINANCE

PROPOSAL FOR A NEW TOWN HALL

Owner: Village of Lancaster

Total Acreage: 0.37

Total Lot Sq Ft: 16,114

4 Central Avenue

Lancaster NY 14086

Lot Acres: 0.06

Lot Sq Ft: 2,614

Tax ID: 145203-104-820-0001-011

Assessed Value: \$12,300

Tax: \$2

6 Central Avenue

Lancaster NY 14086

Lot Acres: 0.05

Lot Sq Ft: 2,175

Tax ID: 145203-104-820-0001-010

Assessed Value: \$9,400

Tax: \$1

8 Central Avenue

Lancaster NY 14086

Lot Acres: 0.12

Lot Sq Ft: 5,227

Tax ID: 145203-104-820-0001-009

Assessed Value: \$24,900

Tax: \$2

10 Central Avenue

Lancaster NY 14086

Lot Acres: 0.14

Lot Sq Ft: 6,098

Tax ID: 145203-104-820-0001-008

Assessed Value: \$28,400

Tax: \$2

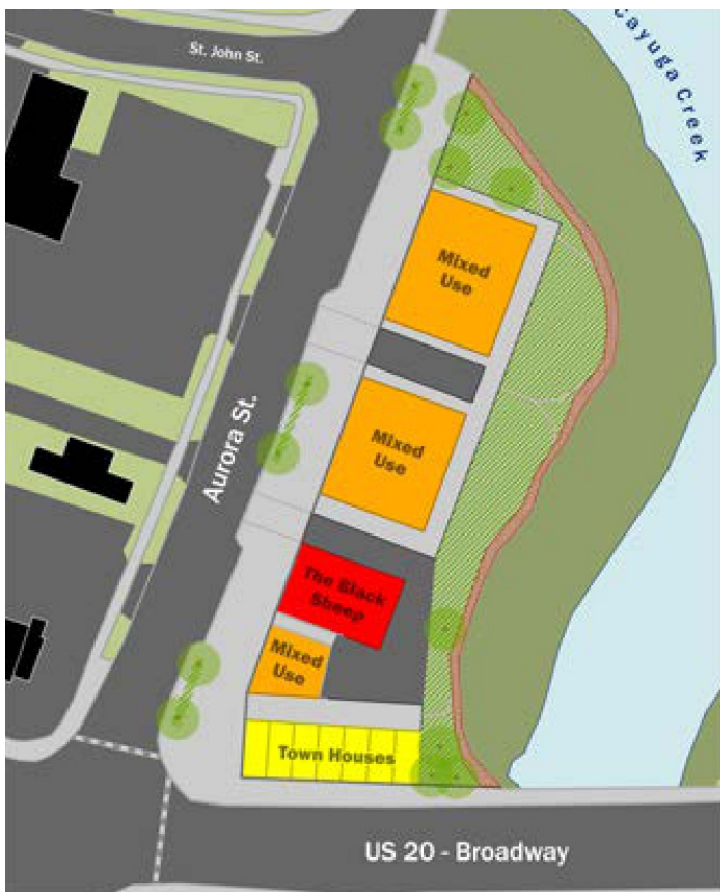
Sources	2023/Year 1	2024/Year 2	Subtotal
Brown Field Clean Up:	\$ 10,000,000.00		\$ 10,000,000.00
New Market Tax Credit:		\$ 310,557.76	\$ 2,422,350.55
Construction Loan:		\$ 1,229,686.80	\$ 1,229,686.80
Total Uses:			\$ 13,652,037.34
Uses			
Land Acquisition Costs:	\$ 1.00		\$ 1.00
Site Development Costs:	\$ 500,000.00	\$ -	\$ 500,000.00
Infrastructure Costs:	\$ 1,100,000.00	\$ -	\$ 1,100,000.00
Direct Costs:		\$ 10,748,400.00	\$ 10,748,400.00
Indirect Costs:		\$ 909,480.00	\$ 909,480.00
Loan Origination Fee:		\$ 8,296.87	\$ 8,296.87
Developer's Fee:		\$ 349,736.40	\$ 349,736.40
Total Sources:			\$ 13,615,914.27
Construction Loan & Equity			
Total Available Sources:	\$ 12,422,350.55		
Total Uses:	\$ 13,607,617.40		
Difference:	\$ 1,185,266.85		
Total Loan Size:	\$ 829,686.80		
Loan Origination Fee:	\$ 8,296.87		
Equity:	\$ 363,876.92		

Calculations by (L. Perry)

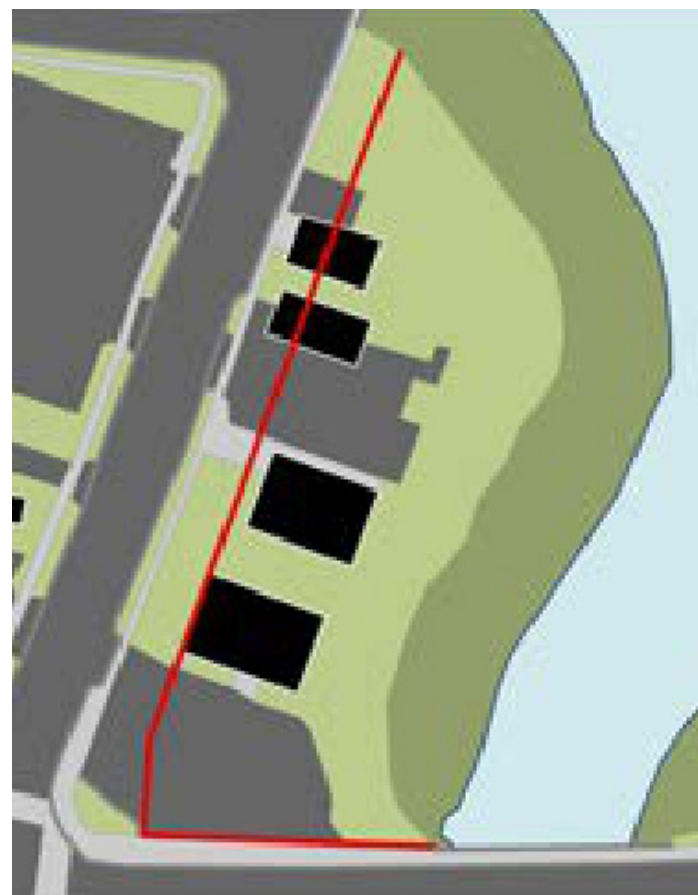
7.3 - THE KNUCKLE A: A MIXED USE DEVELOPMENT PROPOSAL

THE KNUCKLE : PROJECT SITE PROPOSAL

The proposed site will feature a vibrant, and active corner. This will be achieved by creating active store frontage, using pedestrian-prioritized design to create a destination, and by establishing a waterfront trailway that provides the pedestrian with multiple pathways through the corner. This project can serve as the launching pad for pedestrian traffic traveling from the Hub to the Strip and can aid in defining the unique character of the Knuckle. This project will feature mixed-use development and townhouses to create gentle density in the site, and to spur that density increase in the rest of the Knuckle.



Proposed Site Plan (A. Macallini)



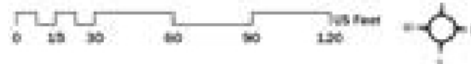
Existing Site Plan (A. Macallini)

New Features

- Trees
- New Gardens
- Tables
- New Creekside Trailway
- Benches
- Access Ramps
- Sidewalks
- Parking Lots
- Patios

Proposed Development

- Townhouses
- Mixed-use
- The Black Sheep Restaurant
- Proposed Setback



Existing Features

- Cross Walks
- Pavement
- Buildings
- Green space
- Parking Lots
- Cayuga Creek
- Creek bed

7.3 - THE KNUCKLE A

A MIXED-USE DEVELOPMENT PROPOSAL

OUR VISION

Our vision board for the proposal consists of widening the right of way and pedestrian walkways. To be accessible to the residents and users of the site and have a wonderful creekside trailway in and around the site which would encourage people to be more pedestrian and bicycle friendly. The outdoor cafe/ restaurant seating area overlooking the creek is an added feature to our proposal along with including townhouses as a part of our proposal to encourage residential and commercial to co-exist with each other.



Proposed Isometric (V. Chavan)

THE KNUCKLE A : FINANCE

THE KNUCKLE

Our proposed development was financed by developer equity, construction debt, and HCR's PLP Program, which helps provide gap financing for projects that fit certain criteria. The rents for our project comply with the 80% AMI for Erie County that HUD established. This allows the project to gain \$90,000 per unit at 0.5% interest to help make it more feasible. All in construction costs were just above \$16 million. The construction phase could take anywhere from 12-24 months to complete, with lease up and stabilization to follow. Following stabilization and a 10-year hold period, the project may be sold between \$9M to \$12M, depending on the current cap rate. In this model, we chose conservative assumptions that would be applicable to a real world scenario.

Unit Mix				
Unit Type	Units	Avg. SF	Rent/Unit/Month	Total Rent/Yr.
1 Bed/1 Bath	27	750 SF	1,100	356,400
2 Bed/2 Bath	16	900 SF	1,500	288,000
Townhome/3 bed	7	1200 SF	1,800	151,200
[Unit Type]		0 SF		-
[Unit Type]		0 SF		-
Gross Potential Revenue	50 Units	861 SF	1,326	795,600

PROJECT-LEVEL RETURNS	
Levered IRR	11.9%
Equity Multiple	3.21x
Profit	7,242,425

Development Budget Summary					
	%	50 /Unit	60,000 Per GSF	60,000 Per RSF	Total
USES					
Land Costs					
Land		10,000	8.33	8.33	500,000
Closing Costs	3.0% of land cost	300	0.25	0.25	15,000
Total Land Costs					515,000
Construction Costs					
Hard Costs		240,000	200	200	12,000,000
Soft Costs	10.0% of hard costs	24,000	20.00	20.00	1,200,000
Contingency	5.0% of construction costs	13,200	11.00	11.00	660,000
Total Construction Costs					13,860,000
Developer Fee	2.5% of Total Costs (less land & contingency)	6,600	5.50	5.50	330,000
Total Project Costs before Financing		294,100	245	245	14,705,000
Financing Costs					
Capitalized Construction Interest					1,564,771
Loan Fee	1.1% of Loan				100,000
Total Financing Costs					1,664,771
Total Uses					16,369,771
SOURCES					
Equity (Total)	LTC 20.0%	Rate			3,273,954
Construction Debt (Total)	80.0%	5.0%			8,595,817
	\$/unit	# units			
PLP Program	90,000	50			4,500,000
Total Sources					16,369,771

Period	Year 0	Year 1	Year 2	Year 3	Year 4	Year 5	Year 6	Year 7	Year 8	Year 9	Year 10	Year 11
Development												
Total Land Costs	515,000											
Total Construction Costs	13,860,000											
Total Financing Costs	1,664,771											
Total Development Costs	16,039,771											
REVENUE												
Rental Revenue												
Rental Market Rent	Rent Growth 2.0%	795,600	811,512	827,742	844,297	861,183	878,407	895,975	913,894	932,172	950,816	969,832
Actual Vacancy & Credit Loss		(47,796)	(48,691)	(49,665)	(50,658)	(51,671)	(52,704)	(53,758)	(54,834)	(55,930)	(57,049)	(58,190)
Retail Market Rent	Annual Bumps 3.0%	270,000	278,100	286,443	295,036	303,887	313,004	322,394	332,066	342,028	352,289	362,857
Actual Vacancy & Credit Loss		(23,600)	(22,248)	(22,915)	(23,603)	(24,311)	(25,040)	(25,792)	(26,565)	(27,362)	(28,183)	(29,029)
Total Rental Revenue		996,204	1,018,673	1,041,605	1,065,073	1,089,088	1,113,666	1,138,819	1,164,561	1,190,908	1,217,872	1,245,471
Other Income												
Parking Income		-	0	-	-	-	-	-	-	-	-	-
Other Income		19,925	20,379	20,832	21,301	21,782	22,273	22,776	23,291	23,818	24,357	24,909
Total Other Income		19,925	20,379	20,832	21,301	21,782	22,273	22,776	23,291	23,818	24,357	24,909
Effective Gross Revenue		1,016,129	1,039,047	1,062,437	1,086,374	1,110,870	1,135,939	1,161,595	1,187,851	1,214,726	1,242,230	1,270,380
OPERATING EXPENSES												
Controllable												
Total Controllable OpEx	Ratio 30.0%	(304,857)	(311,714)	(318,731)	(325,912)	(333,281)	(340,782)	(348,479)	(356,356)	(364,418)	(372,669)	(381,114)
Fixed												
Property Taxes	Millage Rate 1.88%	(143,283)	(146,506)	(149,804)	(153,179)	(156,633)	(160,167)	(163,785)	(167,487)	(171,276)	(175,154)	(179,124)
Insurance		(13,750)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)	(14,025)
Property Management	1.0% of EGR	(10,162)	(10,390)	(10,624)	(10,864)	(11,109)	(11,359)	(11,614)	(11,879)	(12,147)	(12,422)	(12,704)
Capital Reserves	250 per unit	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)	(12,500)
Total Operating Expenses		(484,555)	(495,135)	(505,684)	(516,480)	(527,527)	(538,834)	(550,404)	(562,347)	(574,366)	(586,771)	(599,467)
NET OPERATING INCOME		531,618	543,912	556,753	569,894	583,343	597,106	611,191	625,606	640,359	655,459	670,914
Debt Service												
Debt Service		(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)	(487,886)
Cash Flow from Operations		43,752	56,026	68,867	82,009	95,457	109,220	123,305	137,720	152,474	167,573	183,028
EXIT												
Sale Proceeds	Cap Rate 7.00%											
Sale Price												
Sales Expense	3.0%											
Sale Proceeds												9,296,948

Calculations by (E. Gantley)

7.4 - THE KNUCKLE B: PROPOSAL FOR MIXED USE DEVELOPMENT

A MIXED-USE DEVELOPMENT PROPOSAL



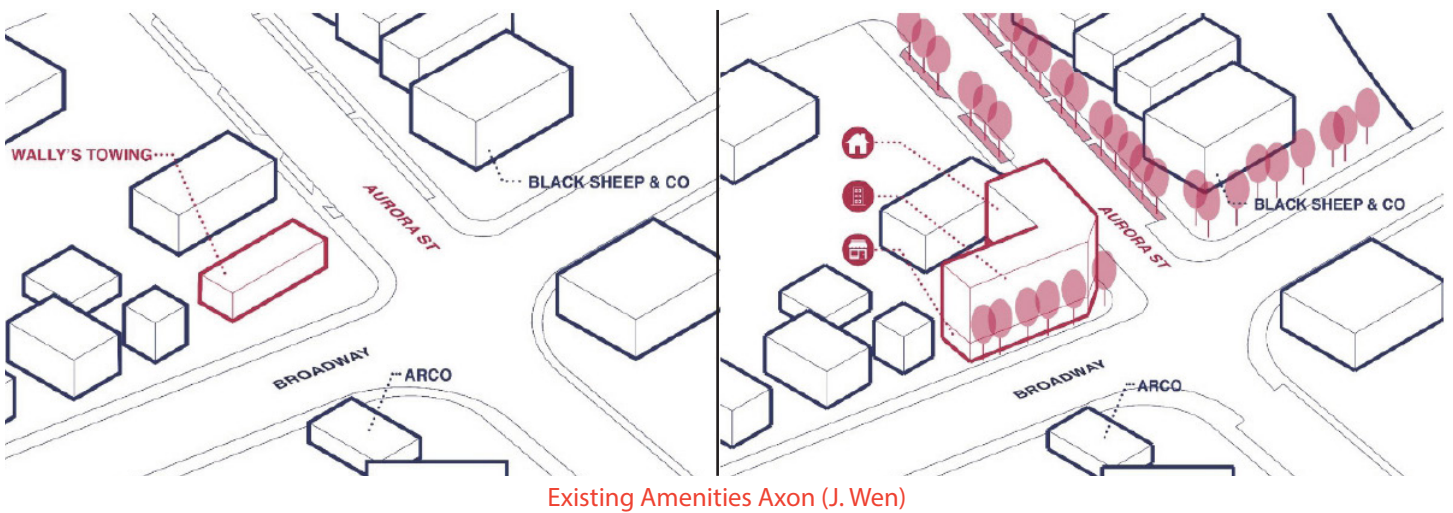
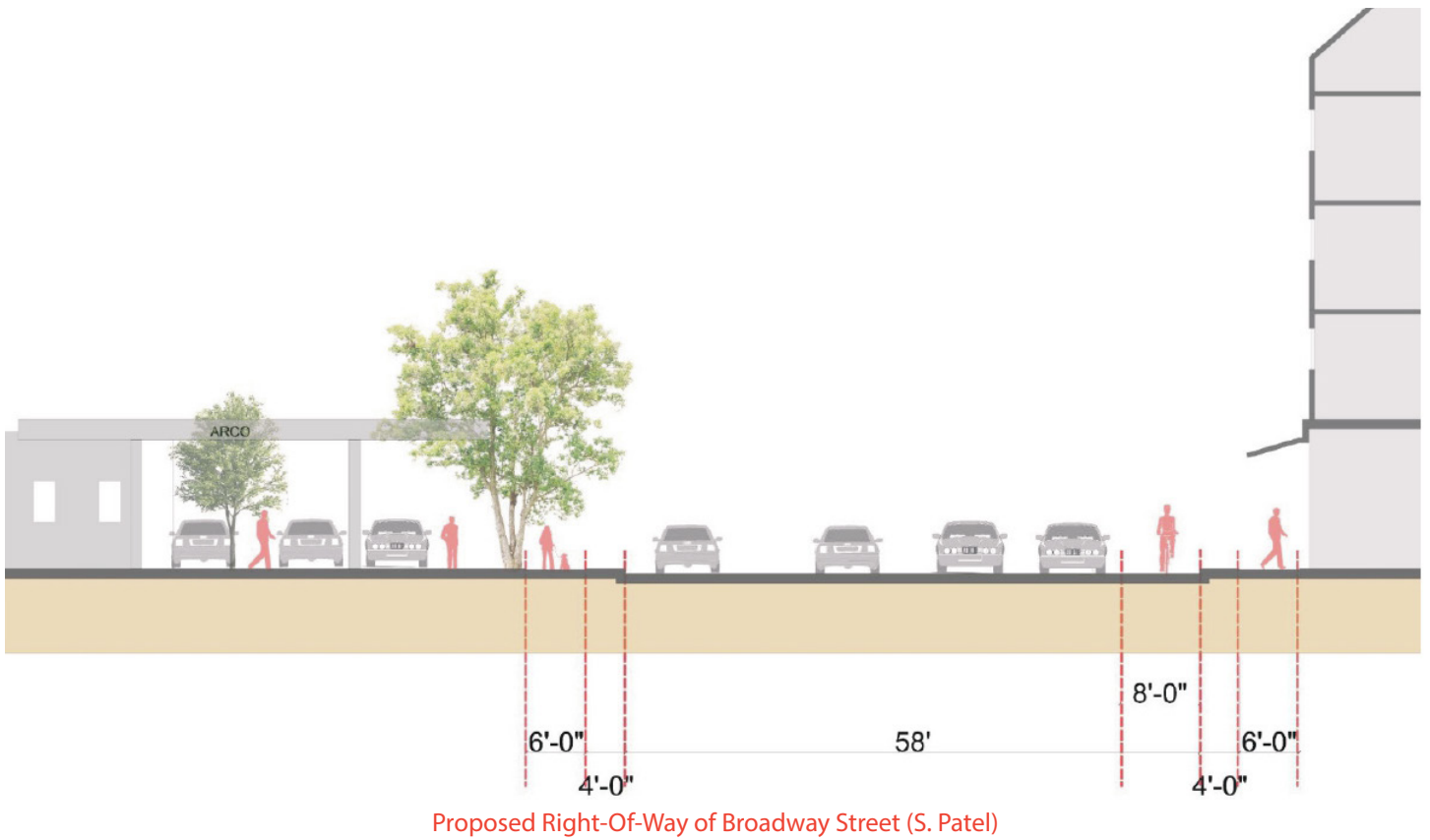
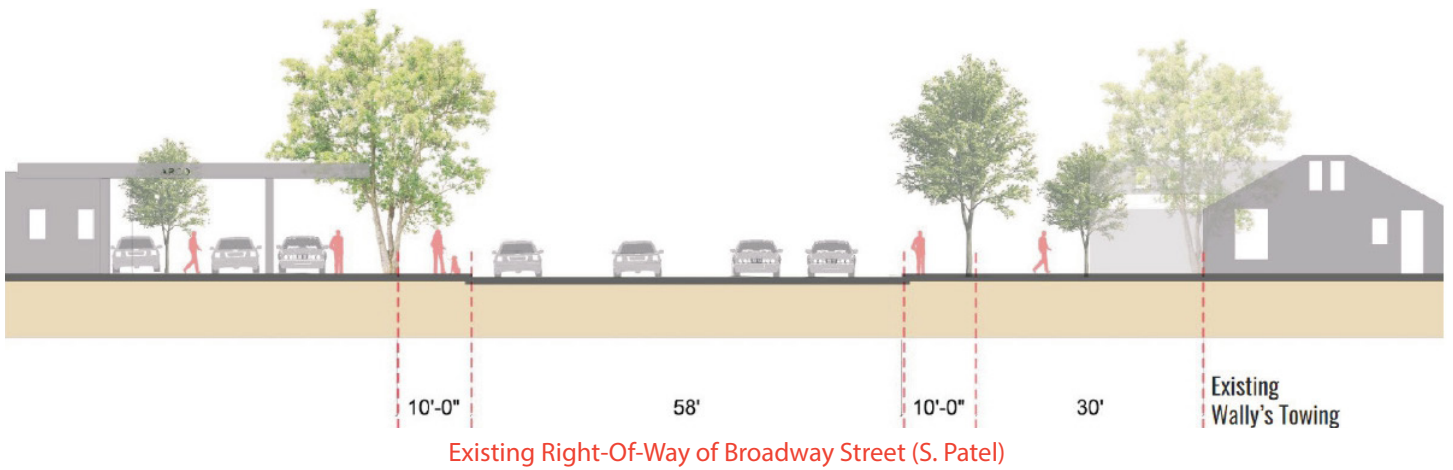
Existing Site Plan (P. Bua)



Proposed Site Plan (P. Bua)

7.4 - THE KNUCKLE B

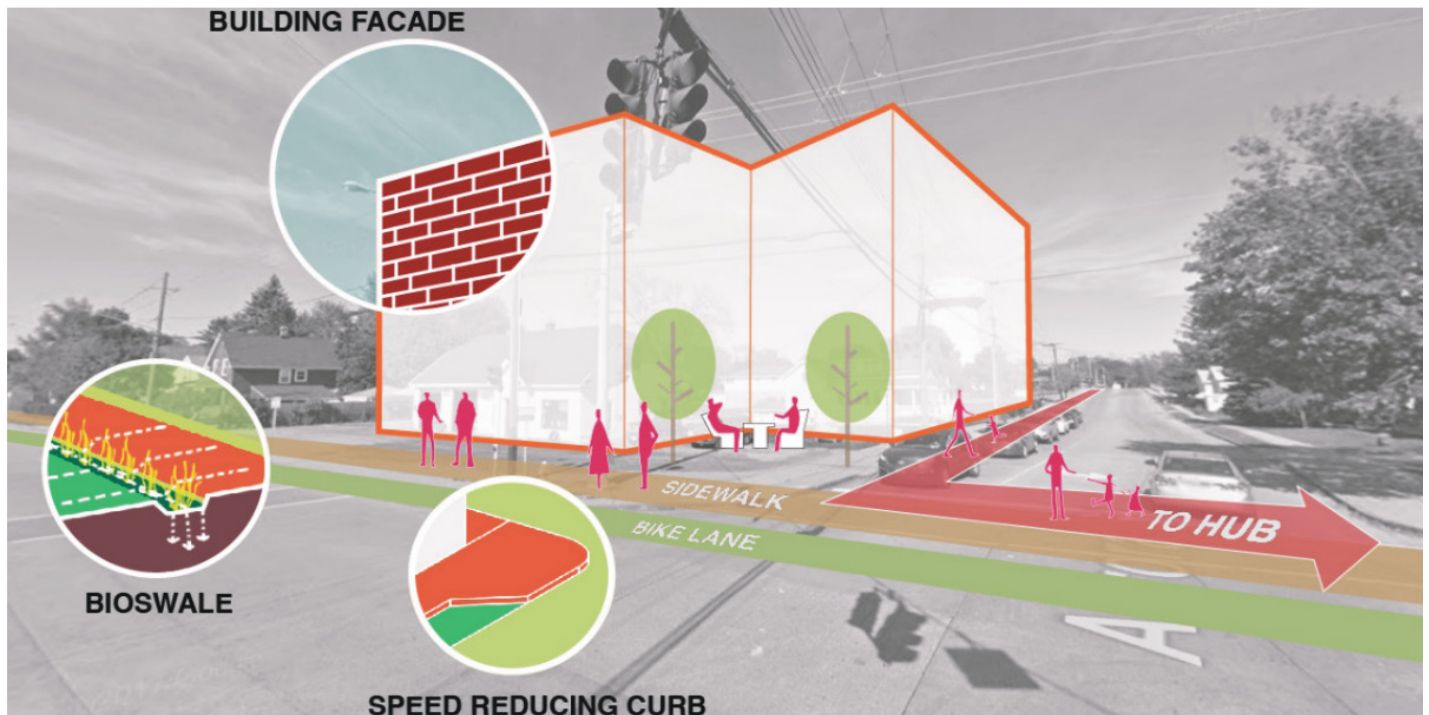
A MIXED-USE DEVELOPMENT PROPOSAL



7.4 - THE KNUCKLE B

A MIXED-USE DEVELOPMENT PROPOSAL

Our proposal also focuses on changes to Aurora St. to slow down traffic. The use of bump-outs allow for a multitude of opportunities including: street parking, more pedestrian friendly sidewalks, ability to include street greenery, and reducing speed of traffic, thus drawing in more potential occupants. The existing Right-Of-Way does not have these features and allows the Wally's Towing Company to exist 30' off the street. Additionally, we are proposing a mixed-use concept which houses five 1250-2000 sq.ft. ground floor store fronts with parking on the street and in the rear of the building, and ten 700-800 sq.ft. housing units per floor above.



Design Features Diagram (J. Wen)



Storefront Program Layout Plan (J. Archilla)



Unit Program Layout Plan (J. Archilla)

7.4 - THE KNUCKLE B

A MIXED-USE DEVELOPMENT PROPOSAL

As discussed earlier, the village contemporary style functions best to tie in the historic character that the Village of Lancaster Center has while also bringing in a new audience with a more modern look. The two facades, along Aurora Street and Broadway Street, would be brick and glazing materials with arched punched apertures and a strong cornice line to relate to the historic character of Lancaster.



Aurora Street Elevation (P. Bua)

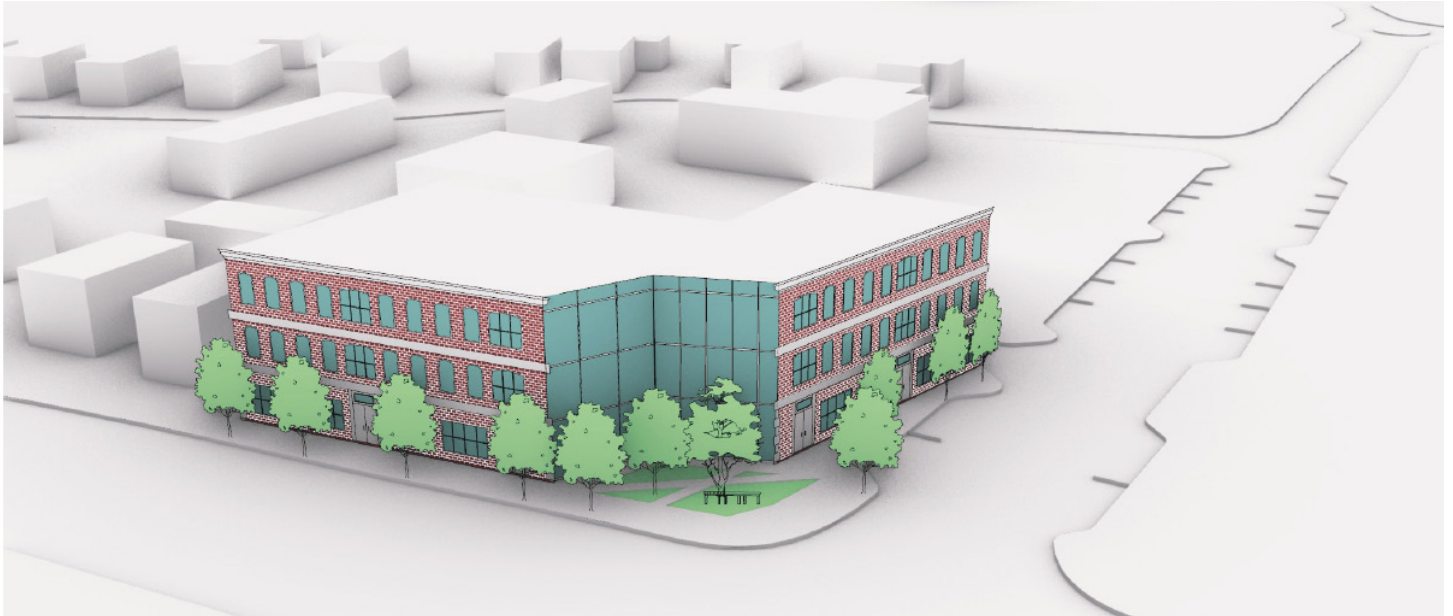


Broadway Street Elevation (P. Bua)

7.4 - THE KNUCKLE B

A MIXED-USE DEVELOPMENT PROPOSAL

Our design proposes grabbing the corner by removing a section of the building at the corner and implementing a pocket park for the residents and users of the storefronts. This creates more life on the street which promotes economic spending and interest in the area.



Aerial Perspective (J. Archilla)



Street-Level Perspective (J. Archilla)

7.5 - THE STRIP A: PROPOSAL FOR A BRANCH BANK AND A PATHWAY

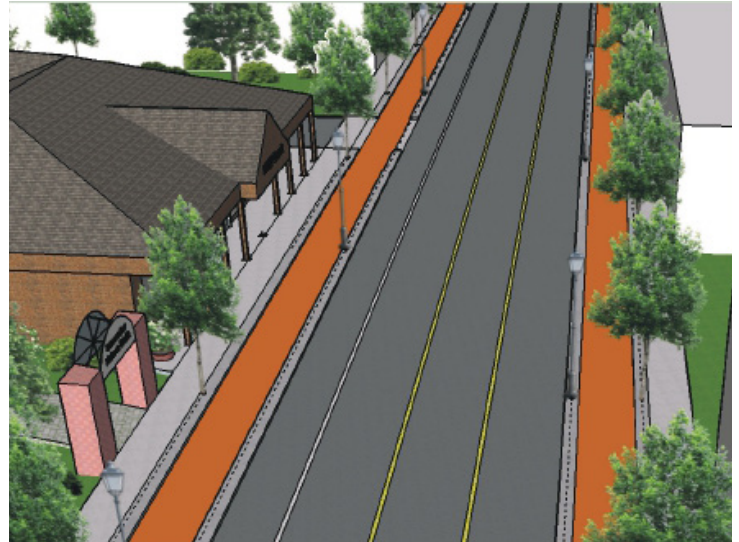
A PROPOSAL FOR BROADWAY

ROAD GUIDELINES

- 3 lane road accommodating different modes of transportation
- A buffered bike lane that is protected by side parking
- A buffer zone located between the bike lane and the sidewalk

GATEWAY GUIDELINES

- Walking path signage towards Cayuga Creek will be located within the green space between the Lancaster Department of Public Works building & M&T Bank
- The proposed gateway signage will be located over Broadway near the western edge of the village.
- Shift the right of way to follow all relevant guidelines.



Proposed Image for Broadway Frontage



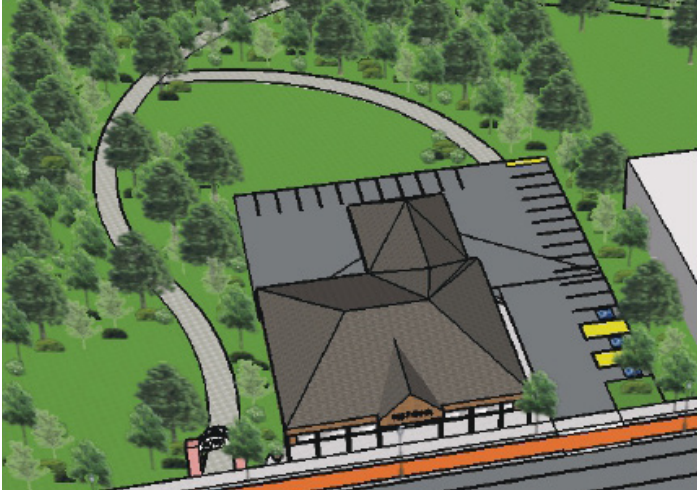
Proposed Gate for Nature Walk

7.5 - THE STRIP A

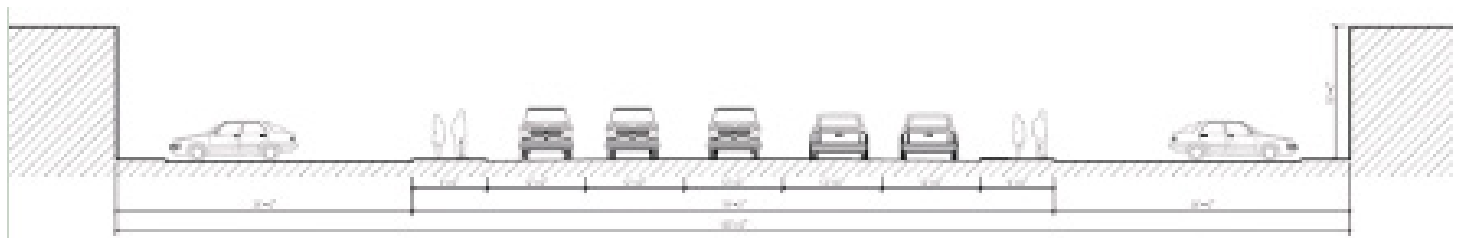
A PROPOSAL FOR BROADWAY

WALKING PATH

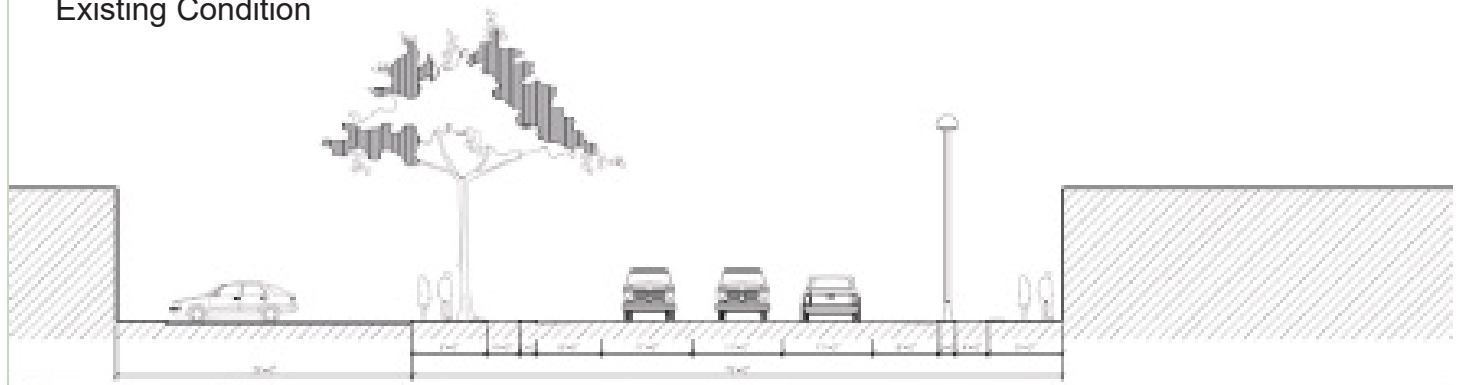
- Create a walking path cutting in between Lancaster Public Works building and M&T Bank that allows access to walking along the berm of Cayuga Creek acting as a pedestrian connector



Renders (N. Rogers)



Existing Condition



Proposed Condition

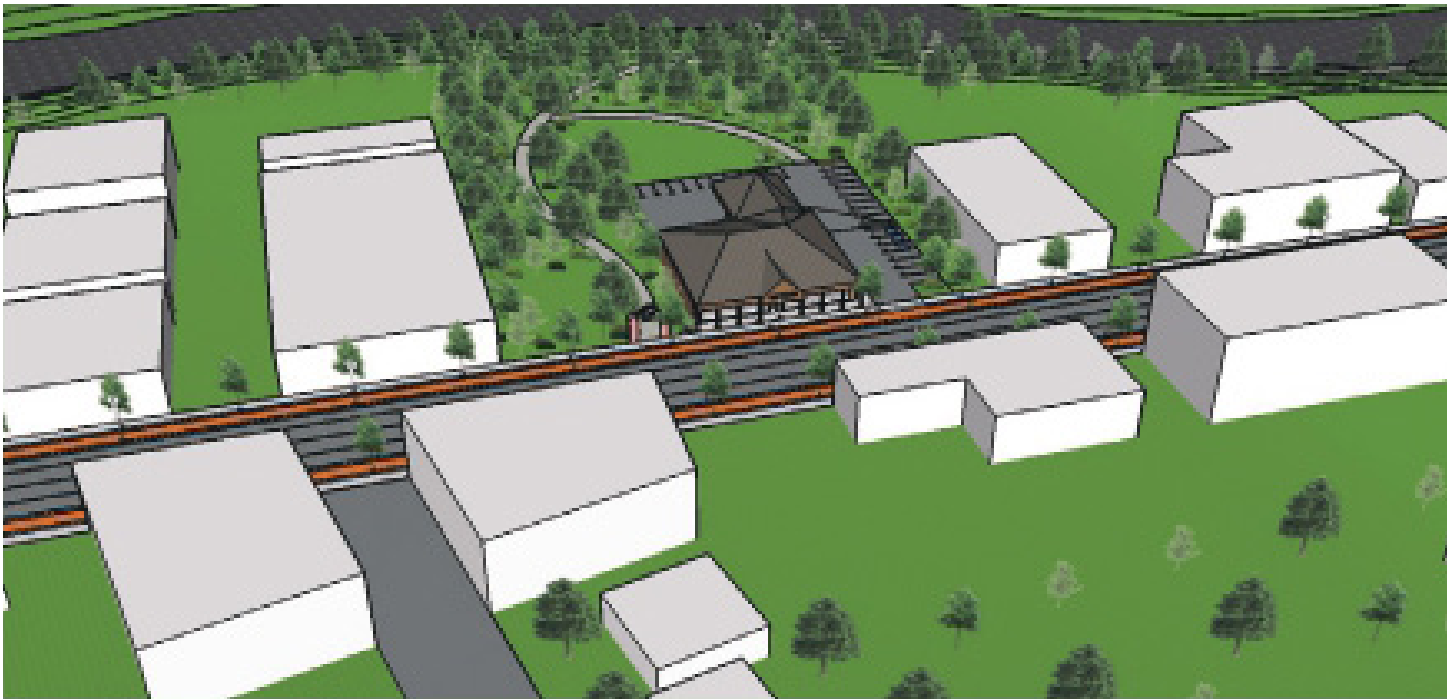
Broadway street section (B. Kwong)

7.5 - THE STRIP A

A PROPOSAL FOR BROADWAY

BANK DESIGN

- The facade of M&T Bank will be fully transparent allowing sunlight to shine through, better working environment with views of the outside, and allowing customers or passersby to see what is happening and what type of facility this is
- Proposing M&T Bank as a 1 story building to limit the amount of parking lots needed and to support additional green space within the



7.5 - THE STRIP A

A PROPOSAL FOR BROADWAY

WALKWAY & SIDEWALK

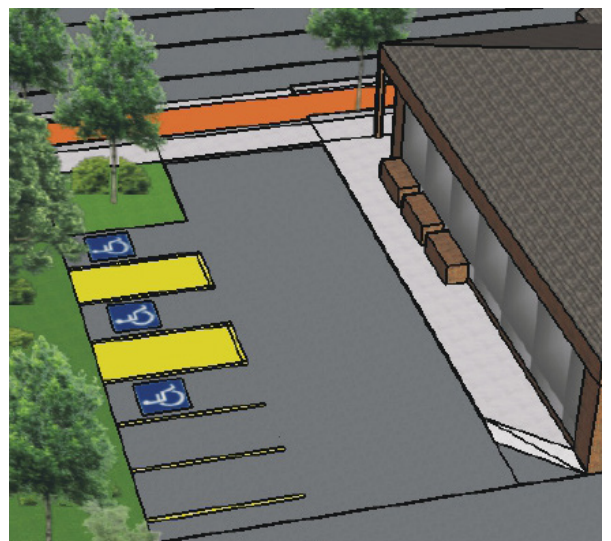
- The M&T Bank will be moved closer to the sidewalk (Built to the edge) allowing closer access for customers
- There will be a walkway that is accessible from the sidewalk and the parking space to the M&T Bank



Renders (N. Rogers)

PARKING

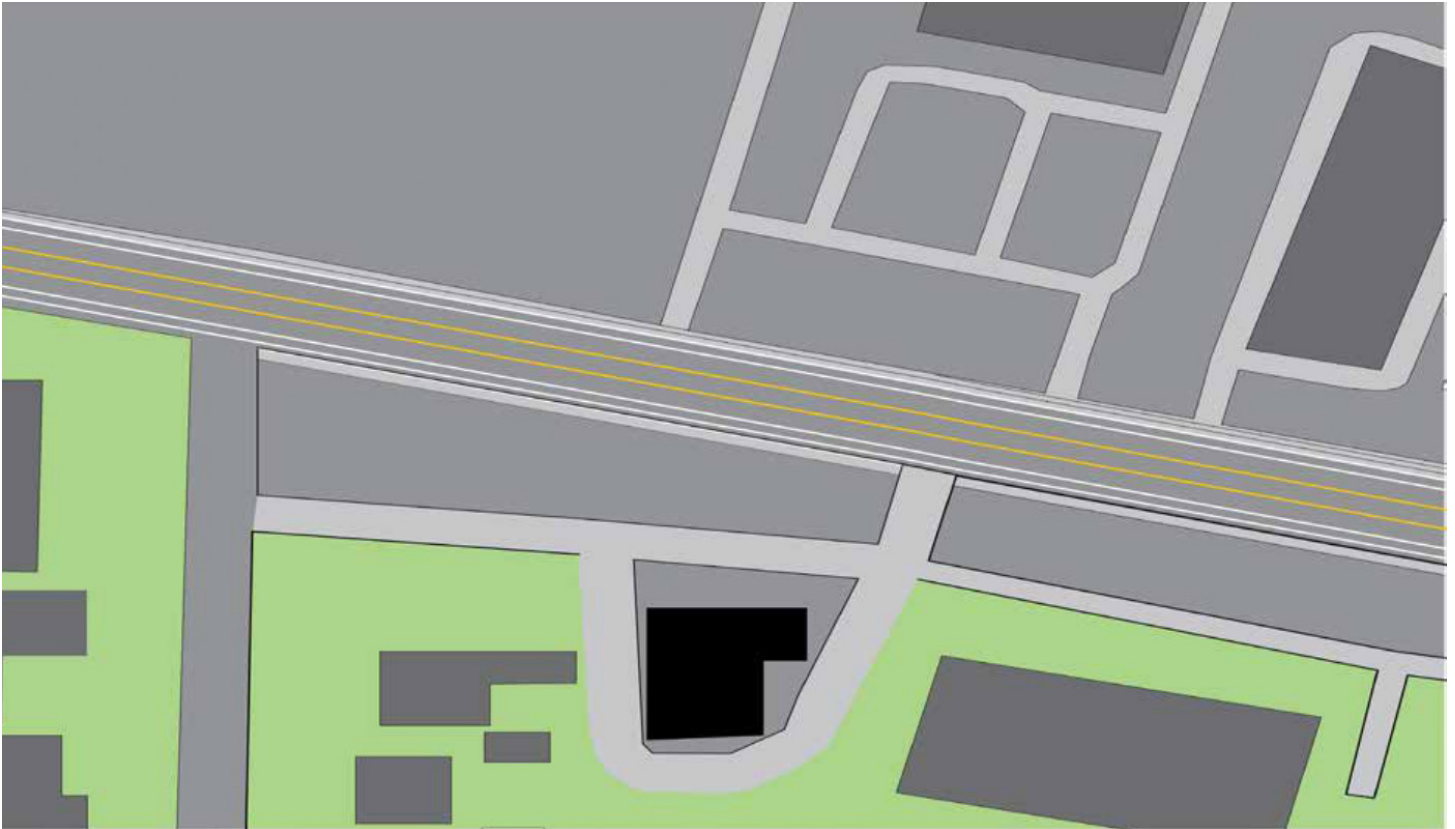
- The parking lot will still exist but it will relocate toward the back of the M&T Bank
- The M&T parking lot will have the entrance facing the street
- Handicapped spaces will be designated closer to the front of the M&T Bank with appropriate signage and maintained in visible condition



Renders (N. Rogers)

7.5 - THE STRIP B:

A MIXED-USE DEVELOPMENT PROPOSAL



Existing Broadway Layout and Building Site Plan (K. Podmayersky)

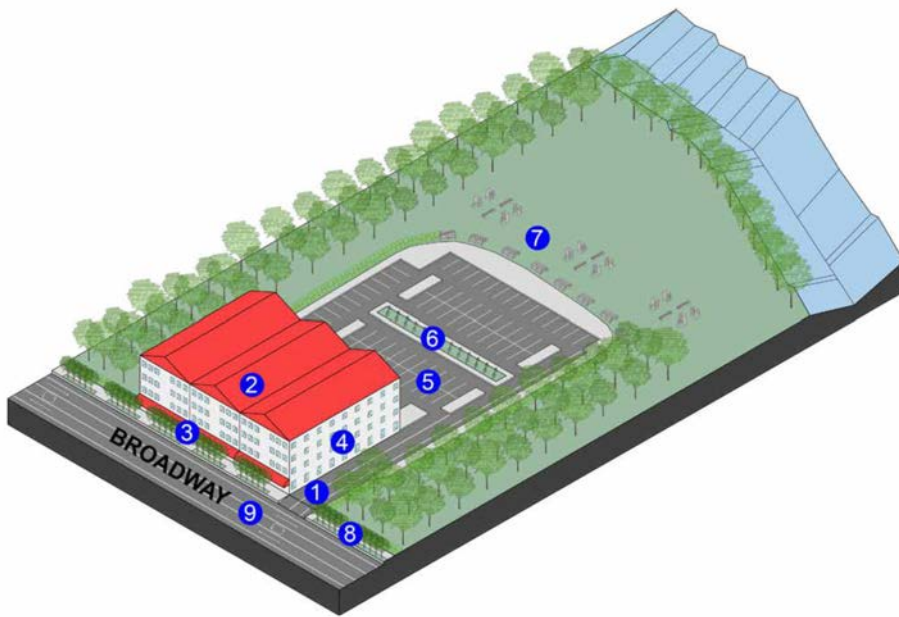


Proposed Broadway Layout and Building Site Plan (K. Podmayersky)

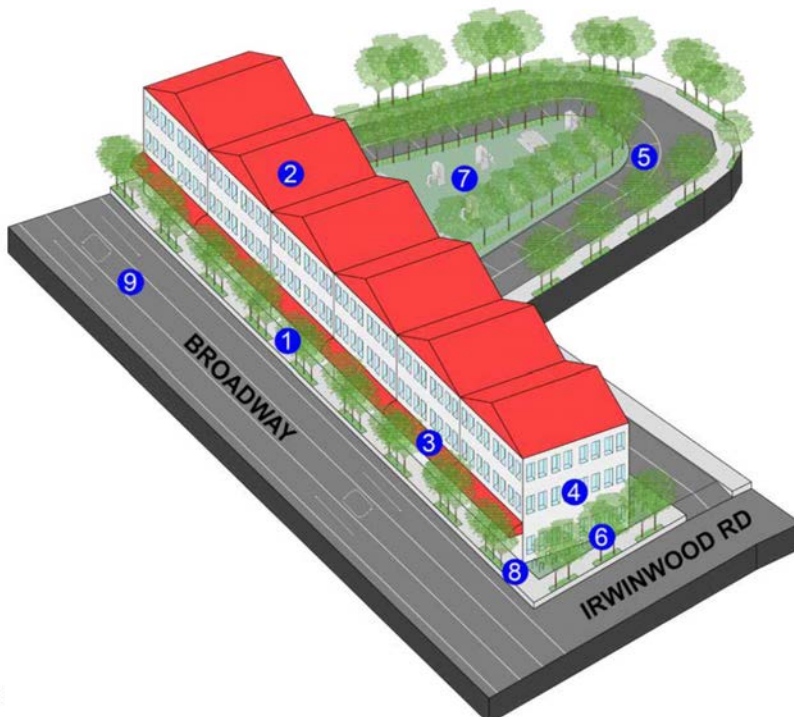
7.6 - THE STRIP B

A MIXED-USE DEVELOPMENT PROPOSAL

The goal of this proposal is to increase the residential space on the strip, which will increase the daily vitality of shops and storefronts in the area. This would be seen through the promotion of mixed-use development. Commercial space is a critical trait of the Strip, but it's strenght can be improved by placing more residents closer to the services they provide. This needs to be paired with human-centric street design, which will improve the flow of pedestrians and make them more inclined to move through Lancaster as a pedestrian. Placing the setbacks of new buildings flush with the sidewalk will bring pedestrians closer to storefronts, and place a greater visual emphasis on pedestrian accessibility. Green space will also improve the appeal of the Strip. This will provide people traveling through the Strip with multiple reasons to stop, not just to shop.



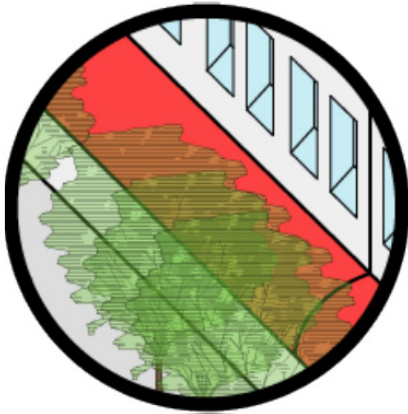
Urban Design Guidelines Aerial Perspective Base Diagram (R. Franqui)



Proposed Building Guidelines Aerial Perspective Base Diagram (R. Franqui)

7.6 - THE STRIP B

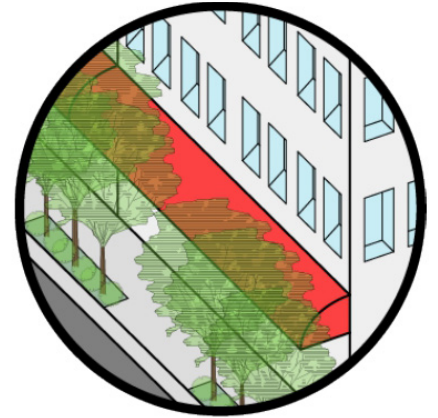
A MIXED-USE DEVELOPMENT PROPOSAL



1. New development should be built to the sidewalk.



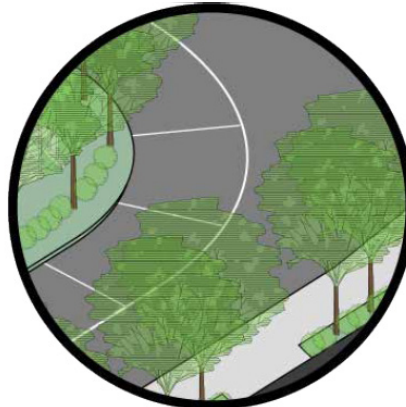
2. New development should be mixed-use with commercial on the first floor and residential on the floors above.



3. Storefronts should be transparent and dynamic, in order to attract pedestrian traffic.



4. New development should not exceed 55 feet in height, 45 feet in width, and no restriction on length.



5. New development with parking lots should place these parking lots in the rear of the building, not on the street.



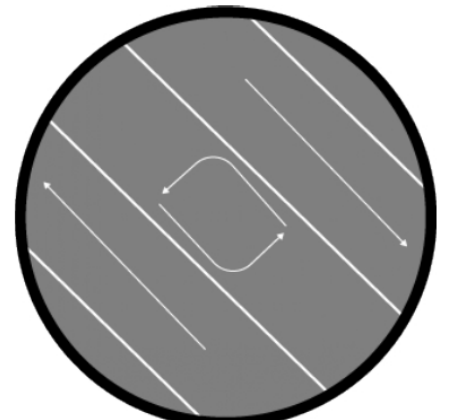
6. New development are required to have bike racks in order to incentivize sustainable transportation.



7. New development are required to build green spaces for their consumers and residents.



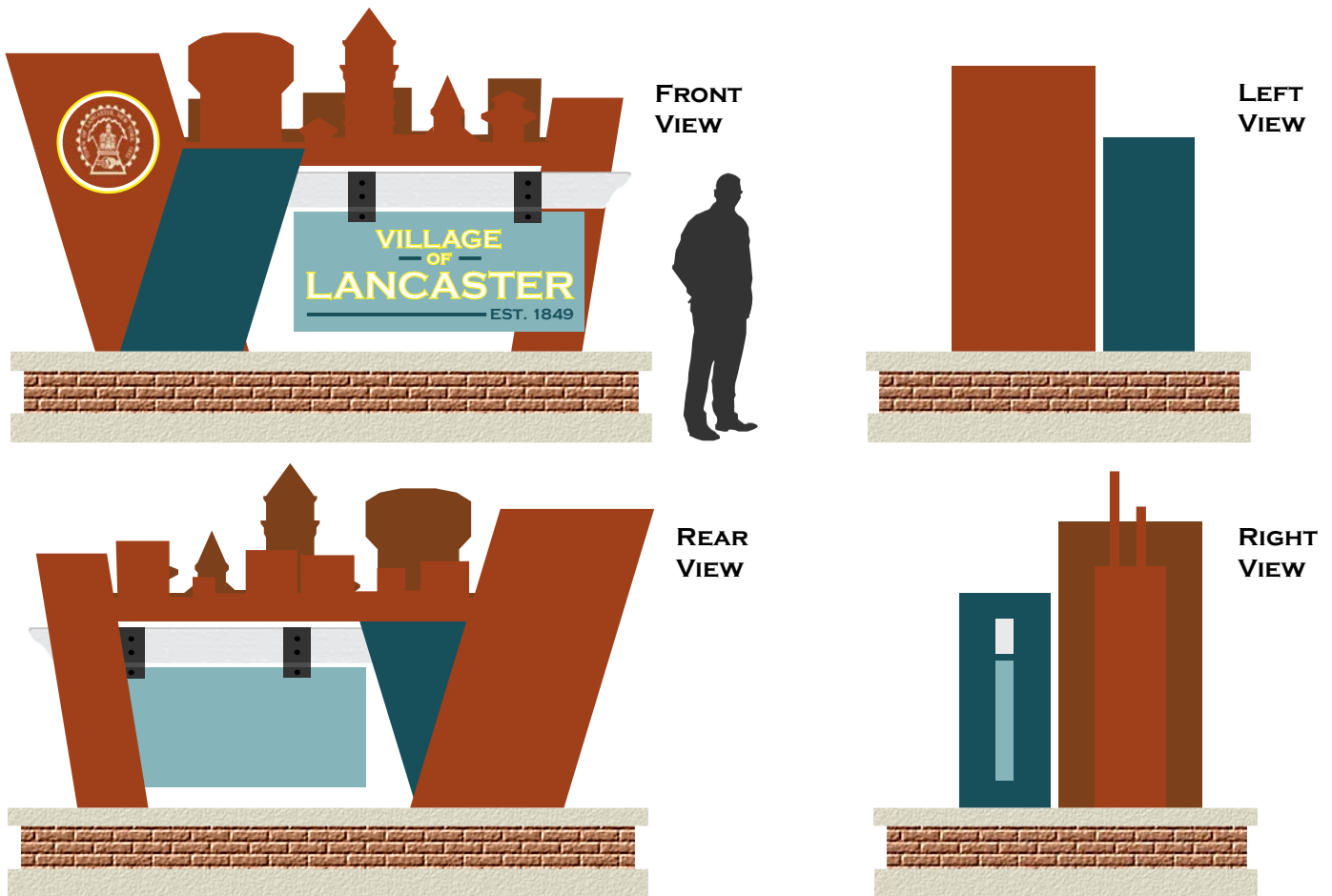
8. Tree lawns are required along the street in order to give a sense of scale to drivers.



9. Streets should have a 10 foot travel lane in each direction, a 10 foot shared turning lane, and 5 foot bicycle lanes going in both directions.

7.6 - THE STRIP B

A PROPOSAL FOR GATEWAY SIGNAGE



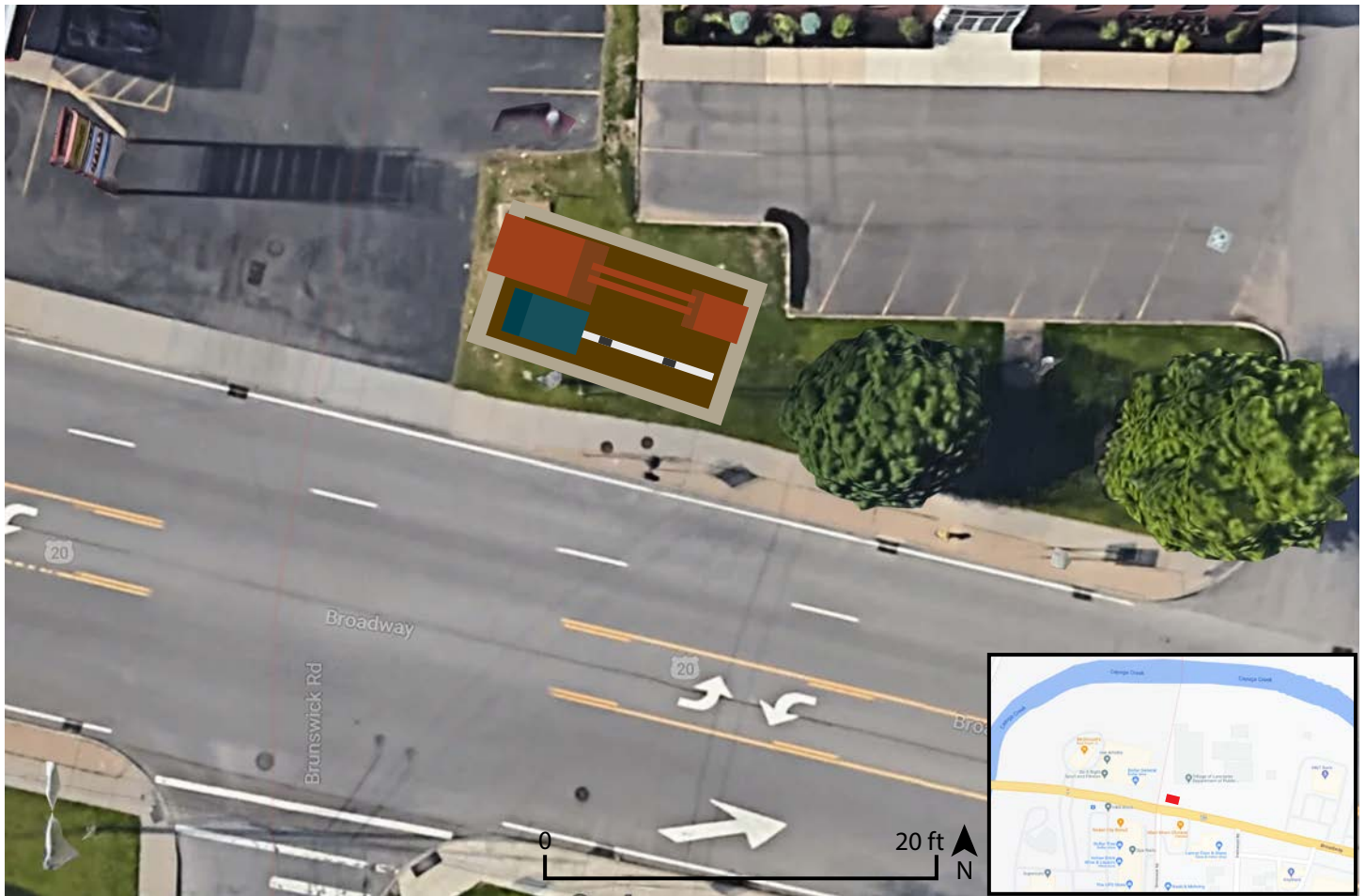
Gateway Signage Design Measurements (N. Barbara)

MATERIAL LIST

- Concrete
- Brick Siding
- ¾" Steel Plates
- ½" Steel Clasps
- 4x6 Wood Planks
- LED Light Strips
- Waterproof Enamel Industrial Paint

7.6 - THE STRIP B

A PROPOSAL FOR GATEWAY SIGNAGE



Gateway Signage Design Renders (N. Barbara)

The Village of Lancaster is undergoing a resurgence in its historic urban form. The redevelopment of West Main Street following decades of urban renewal and the mid-20th century fire. Increased focus on the pedestrian experience has inspired us to reimagine the village through the lens of urban design choices, both in guidelines and in project proposals. Identifying three key target areas within the village allowed us to understand areas of significance within the village that display distinct urban forms, which may need reimagining. These three sites, the Hub, the Knuckle, and the Strip, each signify challenges that the village faces in its urban design, as well as distinct zones to develop a transect model of development, with gradually increasing density eastward along US Highway 20 - Broadway.

The Hub was the first site considered. The proposed design guidelines call for the reflection of the historic architectural styles of the Hub, drawing inspiration from the Opera House and the New York Store's brickwork, ornamentation, and facade. Of the two projects proposed, the gas station site on the northeast corner of Broadway and Central Avenue chose to reflect these principles to create a dense, mixed-use complex to address concerns of missing-middle housing options. On the northwest corner, the proposed project calls for the creation of a style for Lancaster of the future, creating a new municipal center as an anchor point for the Hub.

The Knuckle utilized a different approach to answer the unique challenges of the site. The Knuckle signifies middle-density in the village but utilizes primarily single-use-detached homes and businesses. This limits the density of the site and serves as a potential barrier of movement for pedestrians between the Strip and the Hub. The two project sites of the Knuckle each seek to answer this challenge. The site on the northeast corner of Aurora and Broadway proposes multiple solutions to this challenge, by creating a wider walkway to increase the permeability of the corner and increase public access to the waterfront. It also sought multiple development styles, using mixed-use development and townhouses to enhance the vitality of the corner, and emphasize a key institution in The Black Sheep Restaurant. On the northwest corner of Aurora and Broadway, a similar approach was taken by proposing a mixed-use development that reflected the architectural designs of the hub, creating a sense of transition. Both proposals also called for a reconfiguration of the streetscape of Aurora Street, by creating cut-outs similar to that of West Main Street, and the widening of sidewalks to promote pedestrian movement and congregation.

The Strip sought to solve issues of car-centricity and overly wide right of ways, while placing a greater emphasis on the pedestrian experience, and creating a unique identifying visual for the village. Both projects and guidelines saw to expand upon the newly created bike lanes, by decreasing the lanes of car traffic at the west end of Broadway, and to push the setback line up to the right-of-way. This will create greater vitality and a greater emphasis on the pedestrian experience, while reducing the speed of traffic. The first project proposal saw to create a glass structure for the M&T bank to promote passers-by to stop in. The second project proposal calls for the creation of mixed-use developments along the strip that span higher than projects in the Hub and Knuckle.

Both projects seek to create a sense of place in the strip, which feels placeless as it stands. Both teams also sought to differentiate the Lancaster Strip from that of Depew by creating signage to identify those entering the village.

The solutions proposed at all three sites serve as guidance in ways to reimagine the conditions of the Village of Lancaster. Each project places emphasis on the pedestrian experience in the village, as it has been identified as a critical piece in the success of the village, historically and in the future. A consideration at this scale will allow for the revival of the village's mid-century golden age.

Lancaster has been taking positive steps towards solidifying its urban design. Since 2016, significant focus on improving the urban fabric of the village has re-opened West Main Street. Under the leadership of Mayor Lynne Ruda, the village has been awarded a 4.5 million dollar grant under the state-led New York Forward program, which will be used to continue to maintain and facilitate the historic character of the village. By capitalizing on the historic endowments of the village, is on track to propel itself past the plight of fires and urban renewal. Increased concern for human-scale planning has fostered the conditions to fundamentally change the experience of village-goers. New public waterfront access, improved storefronts, and traffic calming measures create conditions to encourage more vitality in the urban core. This increased concern should be met with consideration to the form and character of the area surrounding downtown. The Knuckle and Strip also serve as important sites to the fabric of Lancaster. Placing an emphasis on accessibility and connectivity will ensure that the great work being accomplished in the Hub permeates throughout Lancaster.

1. Google Earth. (2022). Earth Satellite. Google Earth. HCRSmallBuildingParticipationLoanProgram.<https://hcr.ny.gov/system/files/documents/2022/10/small-buildings-plp-oct-2022-term-sheet.pdf>
2. "HistoricMap-Lancaster,NY-1892."WorldMapsOnline,www.worldmapsonline.com/historic-map-lancaster-ny-1892/. "LancasterNewYork-Stone1869."MapsofthePast,www.mapsofthepast.com/lancaster-city-map-new-york-ny-stew-art-1869.html.
3. NovogradacRentIncomeCalculatorforHUDRentLimits.<https://ric.novoco.com/tenant/rentincome/calculator/z1.jsp> OpenStreetMap. (2021). Village of Lancaster, New York. OpenStreetMap.
4. Rice,B.B.(2022).PhotographyofBuffalo,NY-emersonplacerohouses.BuffaloPhotoBlog.RetrievedDecember 19, 2022, from <https://buffalophotoblog.com/emerson-place-row-houses>
5. "Riverside Park South." Swag Group, www.swagroup.com/projects/riverside-park-south/. "TheWharfDC."WashingtonDC:GreatPatiosforEating&DrinkinginWashington,DC,washington.org/visit-dc/best-restaurant-patios-eating-drinking-wash-ington-dc.
6. U.S.CensusBureau.(2021).Lancastervillage,NewYork.CensusBureau.RetrievedDecember19,2022,from<https://www.census.gov/quickfacts/lancastervilla-genewyork>
7. U.S.CensusBureau.(2021).U.S.CensusBureauQuickFacts:Hamburgvillage,NewYork.U.S.CensusBureau.Retrieved December 19, 2022, from <https://www.census.gov/quickfacts/hamburgvillagnewyork>
8. VisitWashingtonD.C.(2021).BestRestaurantPatiosforOutdoorDininginDC.WashingtonDC.RetrievedDecember 19, 2022, from <https://washington.org/vis-it-dc/best-restaurant-patios-eating-drinking-washington-dc>
9. "Woodlawn Avenue Rowhouses." Buffalo Photo Blog, buffalophotoblog.com/woodlawn-row-houses.
10. Hata,H.(2022,November).ComprehensivePlanning.ARC547-URB565-END565.Buffalo;UniversityatBuffaloSchool of Architecture and Planning.
11. Hata,H.(2022,October).RegulatoryFramework.ARC547-URP565-END565.Buffalo;UniversityatBuffaloSchool of Architecture and Planning.
12. University at Buffalo. (2021). (rep.). Local Government Greenhouse Gas Emissions Study (p. 5). Buffalo, NY.
13. Lane,C.(2018,February21).Howmuchenergydoesasolarpanelproduce?SolarReviews.RetrievedDecember17, 2022, from <https://www.solarreviews.com/blog/how-much-electricity-does-a-solar-panel-produce>
14. (1911). Sandborn Map. Lancaster, New York.
15. BankofAmerica.(n.d.).BankofAmericaFinancialCenterinStatenIsland,NY:RichmondShoppingCenter.Bankof America.RetrievedDecember16,2022,from<https://locators.bankofamerica.com/ny/statenisland/financial-centers-stat-en-island-16765.html>
16. BikePortland.(2015).Cityproposesparking-protectedbikelanesforGatewayRetailDistrict...BikePortland.Retrieved December17,2022,from<https://bikeportland.org/2015/12/10/city-proposes-parking-protected-bike-lanes-for-gateway-retail-district-170012>
17. Google. (n.d.). Google Earth. Google earth. Retrieved December 16, 2022, from <https://earth.google.com/web/@42.90044624,-78.67816195,197.75195357a,500d,35y,0h,0t,0r>
18. Google.(n.d.).Googlemaps.RetrievedDecember16,2022,from<https://www.google.com/maps/place/Lancaster,+NY/@42.9141779,-78.6671958,12.89z/>

-
19. KlokantTechnologiesGmbH.(n.d.).Oldmapsonline.OldMapsOnline.RetrievedDecember16,2022,from<https://www.oldmapsonline.org/>
 20. OpenStreetMap.(n.d.).OpenStreetMap.RetrievedDecember16,2022,from<https://www.openstreetmap.org/#map=4/38.01/-95.84>
 21. Saieh,N.(2009,July31).BranchBankinHettingen/EckerArchitekten.ArchDaily.RetrievedDecember16,2022,from<https://www.archdaily.com/30607/branch-bank-in-hettingen-ecker-architekten>
 22. Signsystems,I.D.(2022,June21).HudsonValleyCommunityCollegeboostsbrandidentitywithfreshnewgatewayandidentificationsignage.HudsonValleyCommunityCollegeBoostsBrandIdentitywithFreshNewGatewayandIdentificationSignage.RetrievedDecember16,2022,from<https://www.prnewswire.com/news-releases/hudson-valley-community-college-boosts-brand-identity-with-fresh-new-gateway-and-identification-signage-301571597.html>

University at Buffalo School of Architecture and Planning

